



REVISED (05/20/16)

DEVELOPMENT PROJECT FILE

for

NEWMAN CENTER / THE MAXWELL ON COLLEGE
(PL150419)

ATTACHMENTS:

1. Location Map
2. Aerial Photo
- 3-64. Letter of Explanation
- 65-72. Previously Approved PAD and Elevations for All Saints Newman Center Student Housing (PL070404)
73. Proposed PAD Cover Sheet for Newman Center / The Maxwell on College
74. Proposed PAD Site Plan for Newman Center / The Maxwell on College
75. Proposed PAD Comparison Table
- 76-77. Site Plan and Enlarged Site Plans
- 78-81. Street and Podium Levels Landscape and Hardscape Plans
- 82-88. Black and White Building Elevations
- 89-95. Colored Building Elevations
- 96-98. Enlarged Elevations and Sections
- 99-102. Building Sections
- 103-111. Floor Plans

- 112. Renderings
- 113. Solar Orientation Study
- 114-116. Shadow Study
- 117. Level 4 Landscape Character
- 118. Site Photos
- 119-130. Parking Study
- 131-133. Traffic Impact Analysis Executive Summary
- 134-139. Public Involvement Final Report Summaries for Neighborhood Meetings
- 140. Action Summary of the January 21, 2016 Historic Preservation Commission hearing for The Maxwell
- 141. Letter of support received April 12, 2016
- 142-143. Letter from Arizona State University received April 28, 2016
- 144. Letter from applicant received May 3, 2016
- 145-148. Excerpt from the Draft Minutes of the Development Review Commission hearing of April 12, 2016
- 149-152. Additional correspondence received
- 153-169. (Received 5-19-16) Follow-up Response Letter from Applicant, including Exhibit C-1 right hand turn lane
- 170. (Received 5-19-16) Revised Conditions Supported by Applicant

THE MAXWELL

PL150419

7TH ST

COLLEGE AVE

**SITE
CC**

UNIVERSITY DR

Zoning
⊞
⊞



Location Map

THE MAXWELL

PL150419



Aerial Map

LETTER OF EXPLANATION

Newman Center/The Maxwell on College

Project Address:

712 South College Avenue
Tempe, Arizona 85281

Cross Streets:

College Avenue and University Drive

Project Location: Northwest corner of University Drive and College Avenue

Parcels: 132-27-356
132-27-107A
132-27-110
132-27-111
132-27-112
132-27-113
132-27-108A

Case Number: PL150419

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13 March 2016

INTRODUCTION

The All Saints Roman Catholic Newman Center Tempe ("Newman Center") and Arizona State University ("ASU") have grown up together. The historic Old St. Mary's was constructed in 1903 just nine (9) years after Old Main in 1894. These days the Newman Center's mission is to serve the ever growing population of ASU; not only the students, but also the professors, supporting professionals and their families. While most of the programs at the Newman Center focus on the college students, the baptisms of newborns and weddings of alumni are weekly events on the grounds. Realizing that ASU had grown to be one of the largest collections of Catholics seeking college degrees in the country, the Newman Center began rethinking its role on the national stage. Three (3) years ago, the Newman Center and the Diocese of Phoenix, searching for a way to offer a Catholic degree in Arizona, talked the University of Mary ("U Mary") into setting up a campus in Tempe across the parking lot from the Newman Center's property. Unlike the Catholic Universities that were later sought out by Mesa and Gilbert, this educational opportunity was established without significant financial support from the host city. U Mary now offers dual degrees with ASU and has both graduate and undergraduate programs. In the same time period the Newman Center established a sorority and fraternity both recognized by ASU. These very successful organizations and are now noticeable elements of life on the grounds.

Throughout this transition, the Newman Center has been working to incorporate a student housing component to its grounds. With a previous development partner, rights to a twenty-two (22) story, 270', 432 bed, 180 dwelling unit tower were approved (and exist) on the Newman Center's property (see Exhibit A - Existing Newman Center Student Housing Entitlements). The Newman Center proceeded with the first phase of those development plans, the new sanctuary, social hall and adoration chapel, and has continued to look for a development partner for the student housing portion. They have also worked with the owners of an adjacent parking lot over which the Newman Center has a ninety-nine (99) year lease, to see if that property could be included in the development plans. Through further conversations, it became possible to include not only the parking lot, but also two (2) adjacent parcels, the parking lot and the shops along College Avenue. As such, this development proposal is a unique partnership between four (4) existing land owners, U Mary's Tempe campus, the Newman Center and our development partner, The Maxwell Tempe, LLC.

EXHIBIT A
EXISTING NEWMAN CENTER STUDENT HOUSING ENTITLEMENTS



twenty-two (22) story - 270' maximum height
432 beds - 180 dwelling units

The Maxwell will provide the retail and student life portions of an expanded faith-focused complex for Newman Center and U Mary's Tempe Campus. It will provide a unique setting for the development of "whole-health" (mind, soul, and body), healthy students at Arizona State University. This will provide high quality housing that can compete with housing opportunities at Catholic universities elsewhere in the country; housing that is safe, protected and provides a focused environment for academic studies. The proposed project will:

- keep retail store fronts along College Avenue
- enhance the walking and biking experience along College Avenue (the main pedestrian route from the Tempe Transit Center to the campus)
- add retail store fronts to 7th Street
- provide structured parking on the parcels north of the alley (206 stalls)
- add offices, classrooms, meeting rooms, a student lounge, and a rectory (3 beds), with restaurant space at the ground level in the north corner of the Newman Center parcel along College Avenue
- add two faith-based student housing towers above the parking structure on the parcels north of the alley (798 beds, 295 units, 245' maximum building height)
- add a sky-park level above the parking structure at the base of the towers
- add a health and amenity ("bridge") level on the eighth (8th) floor of the towers

REQUEST

The Newman Center's property currently is zoned City Center (CC) with a Planned Area Development overlay (PAD) setting requirements for density, lot coverage, landscape area, building setbacks, building height, and reduced parking. The Newman Center's property is also in the Transportation Overlay District (TOD) and has a Historic Overlay District (HOD) designation. The proposal is to:

- 1.) expand the PAD to include the three (3) adjacent property owners, to the north of the alley (currently zoned CC TOD),
- 2.) to replace the commercial uses along College Avenue with new commercial uses (retail/restaurant) on the ground floor and commercial or office space above,
- 3.) add commercial uses along the 7th Street frontage at the ground level,
- 4.) move the student housing portions of Newman Center's existing rights to the northern parcels and, for operational reasons and to offset the high cost of the property, add 366 beds for a total of 798 beds,
- 5.) add a three-bedroom rectory, separate from the student housing towers, to house the priests on-site,
- 6.) continue with reduced parking standards, providing full commercial and guest parking, but very limited parking for the church and student residents,
- 7.) encroach 12' into the 7th Street right-of-way and 15' into the College Avenue right-of-way to provide a shaded pedestrian experience by cantilevering the building above just as ASU's College Avenue Commons building ("CAC Building") has on the northwest corner of College Avenue and 7th Street, and
- 8.) encroach into the alley to provide a pedestrian bridge at the third building level between the Newman Center offices and the south residential tower.

To accomplish this, we are applying to rezone all four (4) owner's properties with a PAD, requesting a re-plot for one of the owner's property to consolidate their lots into a single lot, and applying for Development Plan Review (DPR) for all new development on the properties. The HOD will remain on the Newman Center's property only.

SITE CONTEXT AND SURROUNDING CONDITIONS

The infill site is 1.89 acres and is approximately the eastern half of Block 13 of the original town site for Tempe, bounded by University Drive on the south, College Avenue on the east and 7th Street on the north, located at 712 S College Avenue (College Ave shops – Parcel A), 230 E University Drive (Newman Center – Parcel B), 211 E 7th Street (parking lot – Parcel C), and 704 S College Avenue (Student Book Center – Parcel D) (collectively the “Site”) (see Exhibit B – Land Parcels and Owners). The College Avenue shops and Student Book Center have all been used for decades as retail along the street with parking in the rear via a surface lots off of the alley and 7th Street; while it is underutilized compared to other urban properties in the area, it is the most freestanding retail that the site can support on its own with surface parking. All but the northeastern corner of the Newman Center site is currently utilized as a church, offices and meeting hall. The parking lot is currently a surface parking lot with 68 stalls providing parking for Newman Center staff, guests and hourly for fee parking for the general public. Again while underutilized in this urban area, the site can continue to function profitably as a surface parking facility.

To the east of the Site is ASU’s Foundation Building (a.k.a. the Fulton Center) constructed in 2005. The multi-story building uses a south facing glass wall to enclose the historic forecourt of Old Main and in doing so, eliminated views of A Mountain from the campus and views of the historic church steeple from points east along University Drive. This second view was of major concern to the City’s Historic Preservation Commission who opposed the foundation building for this reason. To the north of the Foundation Building a pedestrian alley cuts through their site from east to west. The alignment of this pedestrian alley is just south of the historic alley cutting through the adjacent blocks. North of the pedestrian alley and still east of the Site is a multi-level parking garage intended to serve the Foundation Building, the liner retail uses and guest of the university. While exposed to surrounding uses on the north, east and south sides, on the west the parking garage is only exposed on the upper levels and is shielded from College Avenue by ground level restaurants and a second level of classroom spaces. A couple of years ago, after the approval of the Newman Center’s twenty-two story tower, solar panels were added to the upper most level of the parking structure. These panels are not screened from the view of adjacent sites or College Avenue. East of the Foundation building is a collection of student housing buildings that fill out the block north of University Drive and south of Veteran’s Way. Currently the University is constructing an approximately eight (8) story, 1600 bed, freshmen engineering dormitory just east of the Foundation Building’s parking structure and south of an existing parking structure off of Veteran’s Way. See Exhibit C – Aerial Site Context

Northwest of the Site is the historic Harrington-Birchett House. North of the Site is ASU’s College Avenue Commons. This building which has retail on most of the ground level with several stories of classrooms above was built in 2014. It’s sustainable east-west orientation, building lines, and cantilevered upper levels are all matched in the proposed Maxwell development. The same architectural team has been used for both buildings to ensure that there is a complementary streetscape created by the space between the buildings.

West of the Site on the north half of the block is a proposed student housing development which received entitlements in 2009 for a 235’, 20 story building with a density higher than that being proposed for the Site. The same property is currently used as commercial space with retail uses along Forest Avenue and U Mary on the upper floors of the building that enters from 7th Street. On the south half of the block, west of the Newman Center, are several commercial lots with a wide variety of uses from restaurant to tattoo parlor – all surface parked

and generally profitable businesses, but underutilized when compared to other urban areas in down town Tempe. One of the lots in this row is currently vacant and cleared.

EXHIBIT B
LAND PARCELS AND OWNERS

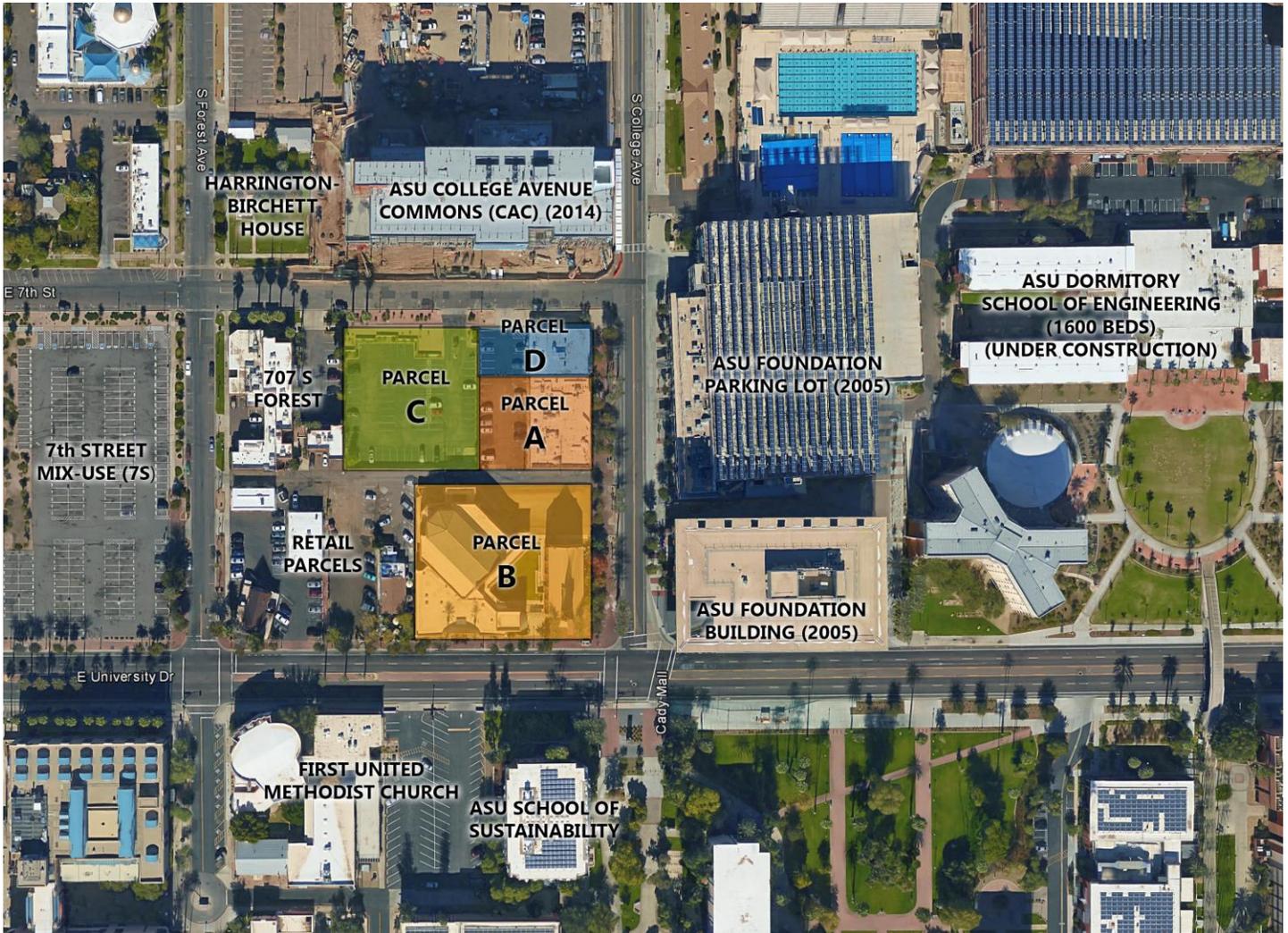


EXHIBIT C
AERIAL SITE CONTEXT



South of the Newman Center is the surface parking lot for the First United Methodist Church with access from University Drive and ASU's School of Sustainability on the east. The view from both of these properties to A Mountain is primarily interrupted at the ground level by ASU's College Avenue Commons building.

The Site is surrounded on the north, east and south sides by MU-Ed zoning which is not limited in height massing or density. On the west and southwest sides, the Site is surrounded by low intensity uses that are likely to redevelop based on existing dense (329 du/ac) and tall (235') entitlements or other similar proposals to come in the near future. The general plan designates this Site and the surrounding properties as High Density-Urban Core (more than 65 du/ac) with those south of the Site as High Density (up to 65 du/ac) (see Exhibit D - General Plan - Projected Density). The Site and the surrounding properties are also designated Mixed Use with Educational land use designation on the property southwest of the Site (see Exhibit E - General Plan - Projected Land Use). The Downtown Building Heights Concept Study designates the north half of the site and all of the Properties to the north and west as Urban Center area with a height called out as 300' max. It designates the Newman Center property with its existing 270' entitlement and the properties south of University Drive as Civic/Cultural area with a height called out as 75' max. See Exhibit F - Downtown Building Heights.

EXHIBIT D
GENERAL PLAN – PROJECTED DENSITY

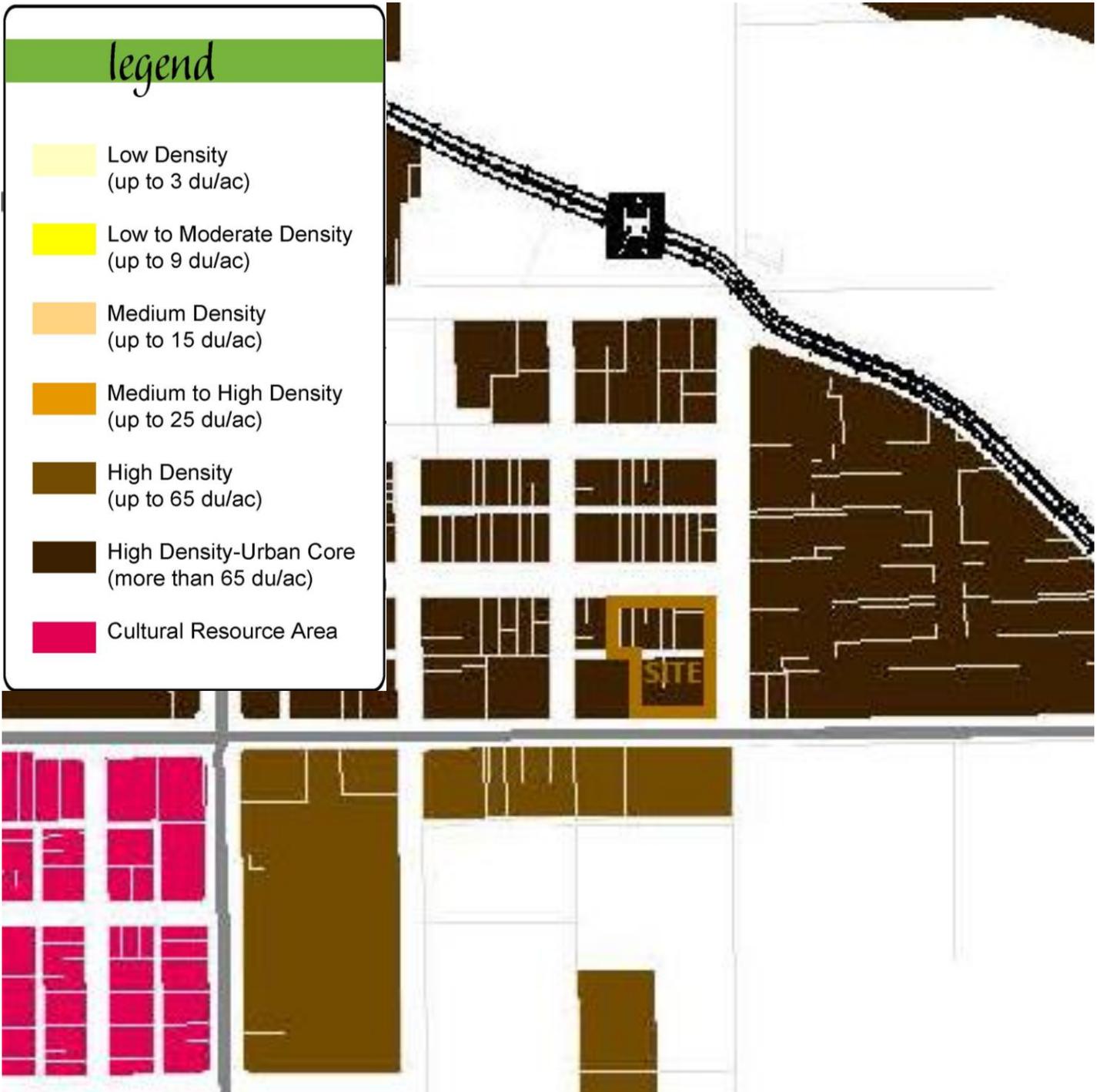


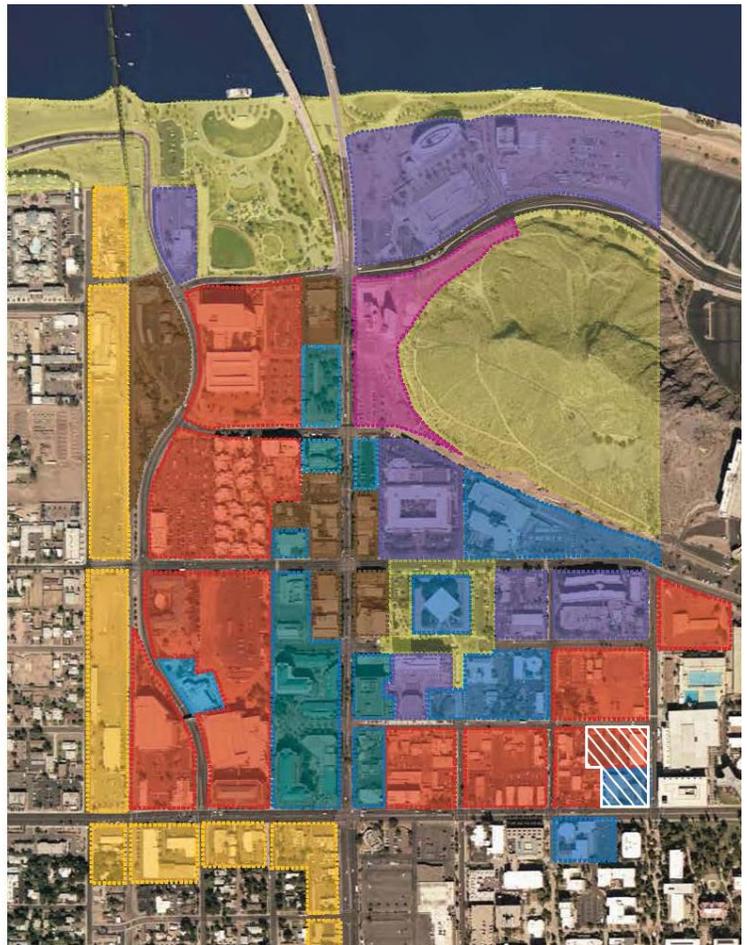
EXHIBIT F DOWNTOWN BUILDING HEIGHTS

legend symbol	area	height
	NEIGHBORHOOD BUFFER	50' max.
	HERITAGE CORE	75' max. (15' stpbk. @50' height)
	CIVIC/CULTURAL	75' max.
	MILL AVENUE CORRIDOR	150' max. (15' stpbk. @50' height; 10' stpbk. @ 75' height)
	URBAN- PARK TRANSITION	200' max. (20' stpbk. @75' height)
	URBAN CENTER	300' max.
	HAYDEN FERRY SOUTH SPECIAL STUDY AREA	See specific guidelines*
	PARK/PRESERVE	

* "Hayden Ferry South Development Guidelines" were accepted by the Rio Salado Advisory Commission in March 2000, accepted by the Tempe City Council in August 2000 and amended by development agreement in April 2001.

Accepted 6 April 2006 (Central City Development Committee of the Whole)

**CONCEPT STUDY
DOWNTOWN BUILDING HEIGHTS**



DEVELOPMENT CONCEPT

This is a unique opportunity to create a faith based college community anchored by the successful social, support and educational programs of the Newman Center and U Mary. The two (2) organizations are committed to supporting religious education and personal growth as part of the ASU academic experience. This partnership will be able to offer a lifestyle and community similar to that sought out by students and their families at eastern religious colleges. This will not be a copy of the older organizations, but a re-invention of what they provide taking from their best practices and adding a partnership with ASU; together developing a new element of the New American University.

The buildings will be a mixed-use expansion of the Newman Center and U Mary grounds to include the three (3) parcels north of the alley. This will allow the organizations to enhance their offices and classroom space on their existing property and add a 798 bed, faith-based student housing community in a setting that integrates learning, sharing of cultural ideas, and integrates faith in the collegiate conversations. The project will do this through some un-conventional ways like incorporating a coffee shop/public house in the College Avenue frontage behind the historic church, integrating out-door classroom space into the Sky Park, and providing ground level retail/restaurant space to encourage interaction with the greater community at the front door of this one.

The buildings have been cleverly designed to provide transition and layering between the historic brick buildings existing along University drive (Historic Old St. Mary's, the new sanctuary and social hall, and the ASU Foundation Building) and the very contemporary College Avenue Commons building recently constructed by ASU just north of 7th Street. The buildings are sustainably oriented in the east-west alignment which also minimizes their presence on the strong pedestrian corridor – College Avenue. Rather than a solid block that cuts off the pedestrian alley corridor and fills the entire block with internal private courtyards, the layered towers open up views from College Avenue up into the project. This design links upper level semi-private open spaces to the public ground level while still providing the privacy and security necessary for student housing. The south residential tower meets College Avenue providing a very private open space for the resident pool on the western side. This intentionally separates the pool from the public street to reduce the “party” qualities of the contemplative outdoor space. The north residential tower slides to the west, sitting 70' off of College Avenue, opening up the corner and allowing the buildings to be off-set from one another. The placement of the north tower again isolates the pool from the public realm along 7th Street. See Exhibit G – Proposed Site Plan and Exhibit H – Proposed College Avenue Composite Elevation.

The Newman Offices building will include a coffee shop/public house on the ground level along College Avenue north of the historic church. The ground level will also house the reception office and student lounge off of the existing the courtyard lawn as well as offices for the student ministers. On the second level will be offices, meeting rooms and classrooms for the Newman Center. On the third level will be offices, meeting rooms and classrooms for U Mary and a pedestrian bridge connecting across the alley to the multi-purpose spaces in the south tower of the Maxwell. On the fourth level, will be the rectory with three bedrooms for the priests to live on-site.

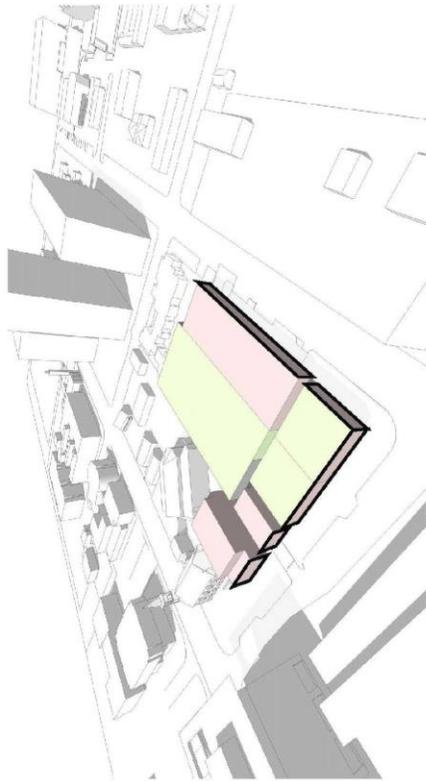
EXHIBIT G
PROPOSED SITE PLAN



EXHIBIT H
PROPOSED COLLEGE AVENUE COMPOSITE ELEVATION



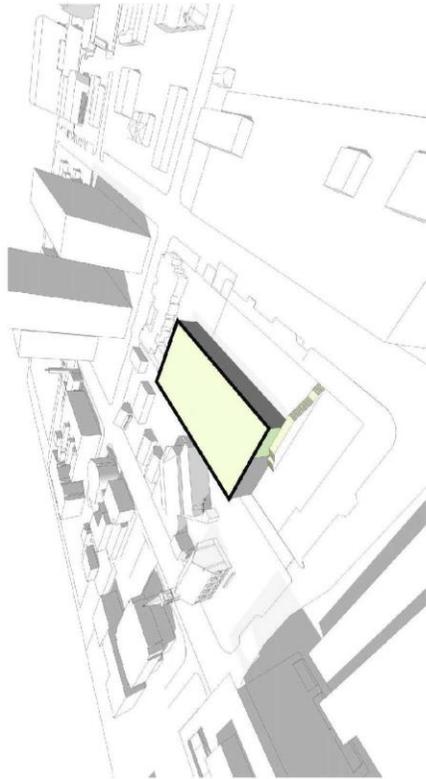
EXHIBIT I
MAXWELL DESIGN CONCEPT ELEMENTS



02 ENGAGE PEDESTRIAN AT STREET



04 VERTICAL RESIDENTIAL COMMUNITY



01 ELEVATED AMENITY PARK



03 AMENITY BRIDGE / LINK

The Maxwell (student residence) portion of the Site (see Exhibit I – Maxwell Design Concept elements) has a parking structure with one (1) underground level and three (3) levels above grade in the southwest corner with vehicular access from 7th Street and the alley. On top of the garage is the Sky Park, an outdoor contemplative setting connecting the two residential towers at the 4th level (see the **green** element on Exhibit I – Maxwell Design Concept Elements). See Sky Park Section to follow.

Ground level retail lines College Avenue and 7th Street, opening internal uses to the street to engage pedestrians and activate the streetscape (see the **pink** and **red** elements on Exhibit I – Maxwell Design Concept Elements). The retail has been designed so that it could be restaurant uses with supporting grease traps and dining spilling out into the right-of-way with café tables and chairs. These outdoor dining spaces along College Avenue and 7th Street may be enclosed by low walls or fences as required for the service of alcoholic beverages. The retail space entirely screens the parking garage from the 7th Street and College Ave streetscapes. Along College Avenue above the ground level retail, the second and third levels have office/retail/restaurant space and multi-purpose rooms. The multi-purpose rooms are designed to support events, conversation and learning in the faith-based residential community as well as providing additional space for U Mary to utilize and offer programming to residents and their student body. The office space may be leased to outside companies, organizations or to the Newman Center and U Mary as their programs grow. The second level along 7th Street is designed to house office or retail/restaurant space.

The commercial spaces that extend from the corner of College Avenue and 7th Street have a one and a half floor height to both commercial levels (16' typically). The commercial spaces on either side of the ally have a typical floor to floor height and have three (3) levels while the corner has two (2) in the same height. The building levels re-unite on the fourth level. The two-level commercial uses have a separate lobby accessed from 7th Street to provide an arrival for uses on the second level. A dramatic stair case also rises from the corner to the 4th level Sky Park along the commercial lobby, providing second commercial level access on the way up.



Artist Rendering of College Avenue Retail/Restaurant Uses from Southeast Corner of the Newman Offices Looking North



Artist Rendering of College Avenue Retail/Restaurant Uses from Northeast Corner Looking South



Shaded Sidewalks on 7th Street and College Avenue to Complement the Existing ASU CAC Building



Artist Rendering of Retail/Restaurant Uses along 7th Street Looking West

Surrounding the community spaces, the Sky Park on the 4th level and the Resident Lounge on the 8th level are four (4) residential blocks that make up the north and south residential towers (see the **light blue** elements on Exhibit I – Maxwell Design Concept Elements). It is contemplated that these blocks may separate under-graduate from graduate students and be restricted to single sexes on some levels. These spaces are intended to have quality, contemporary, contemplative and simple interiors. Each unit is set up apartment style with one (1) kitchen and living room in each unit and the towers contain a mix of single, two (2) and four (4) bedroom units, typically leased by the bed. Pets are not permitted or accommodated at this time. The four (4) residential blocks are linked together by the community spaces, and both towers open to each other on the 4th and 8th level.

The residential communities are accessed by narrow lobbies on College Avenue and 7th Street. The College Avenue lobby will be the main lobby and includes building security on the ground level. This lobby also provides access to the upper levels of the three-story commercial/multi-purpose space portion of the building. The 7th Street lobby will be key card accessed and un-manned.



Artist Rendering of the Collage Avenue Residential Lobby



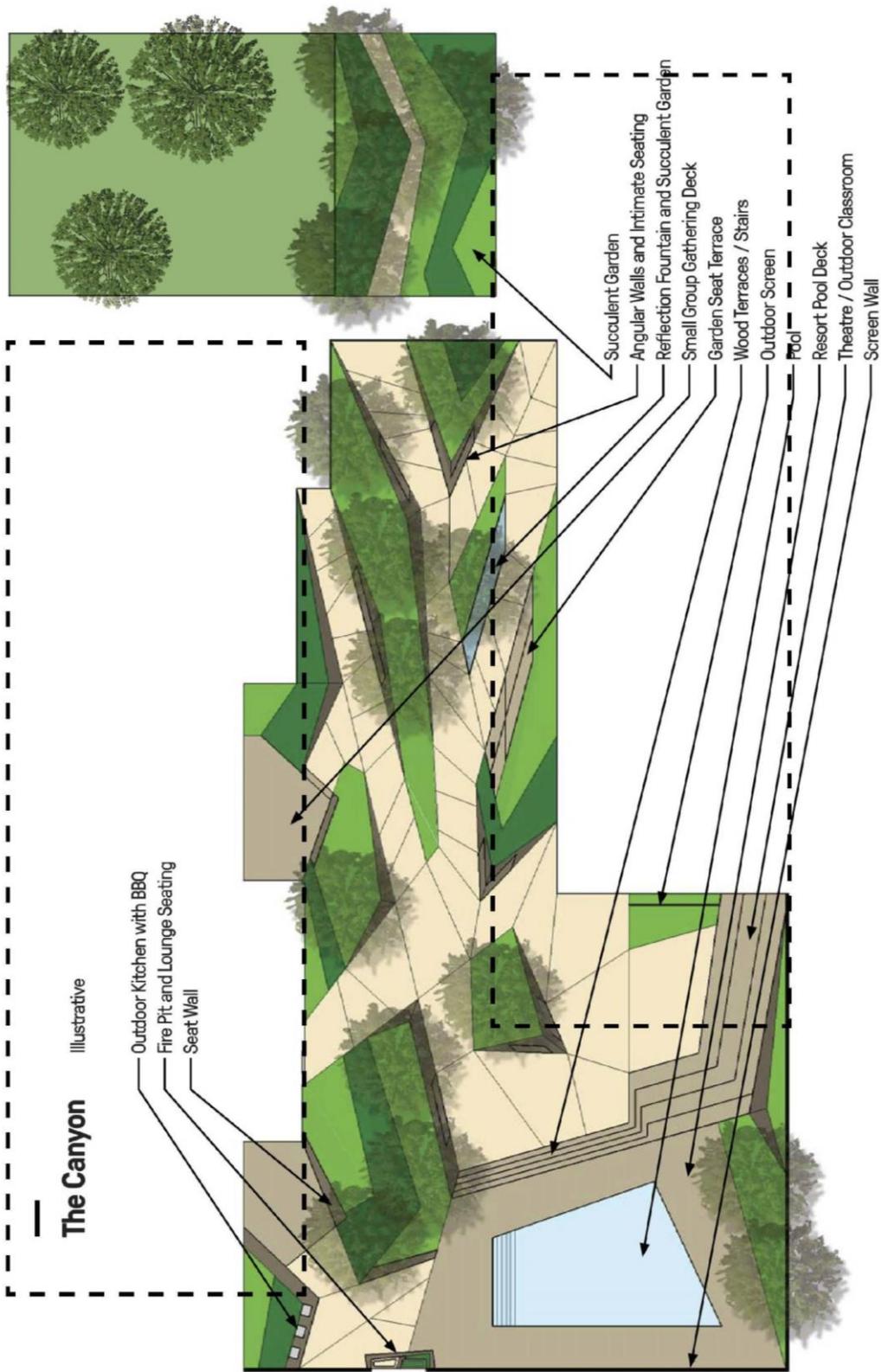
Artist Rendering of Student Housing Unit's Living Room

SKY PARK

The Sky Park on the 4th level provides one of the four (4) main open spaces on the grounds and the primary semi-private open space for the residents of the Maxwell towers (see Exhibit J – Proposed Sky Park). The park is designed with outdoor cooking and pool amenities on the west end, with outdoor classroom and breakout spaces in the middle, and small intimate outdoor spaces on the east end. At the corner the Sky Park has a separately accessed, roof top grove. The grove may have a commercial restaurant use or be leased for private events. This corner element may also include a structural shade canopy for restaurant like uses. The garden setting of the Sky Park is filled with useful plants as well as those found in the landscapes of important historic faith stories. There is no large party deck or open event space, jumbotron or grand sun deck as this space is designed to be more contemplative. Inspired by ASU's Barrett Honor's College, the multi-purpose spaces that surround the Sky Park open into the park's landscape so that walking through the community spaces one can always see that this is an academic environment

The other three (3) main open spaces are also available to residents of the Maxwell, but have other primary uses – the courtyard lawn at the center of the existing Newman Center grounds, the plaza at the steps of Old St. Mary's, and the retail/restaurant activated streetscapes of Collage Avenue and 7th Street.

EXHIBIT J
PROPOSED SKY PARK



COLLEGE AVENUE STREETScape AND MASSING

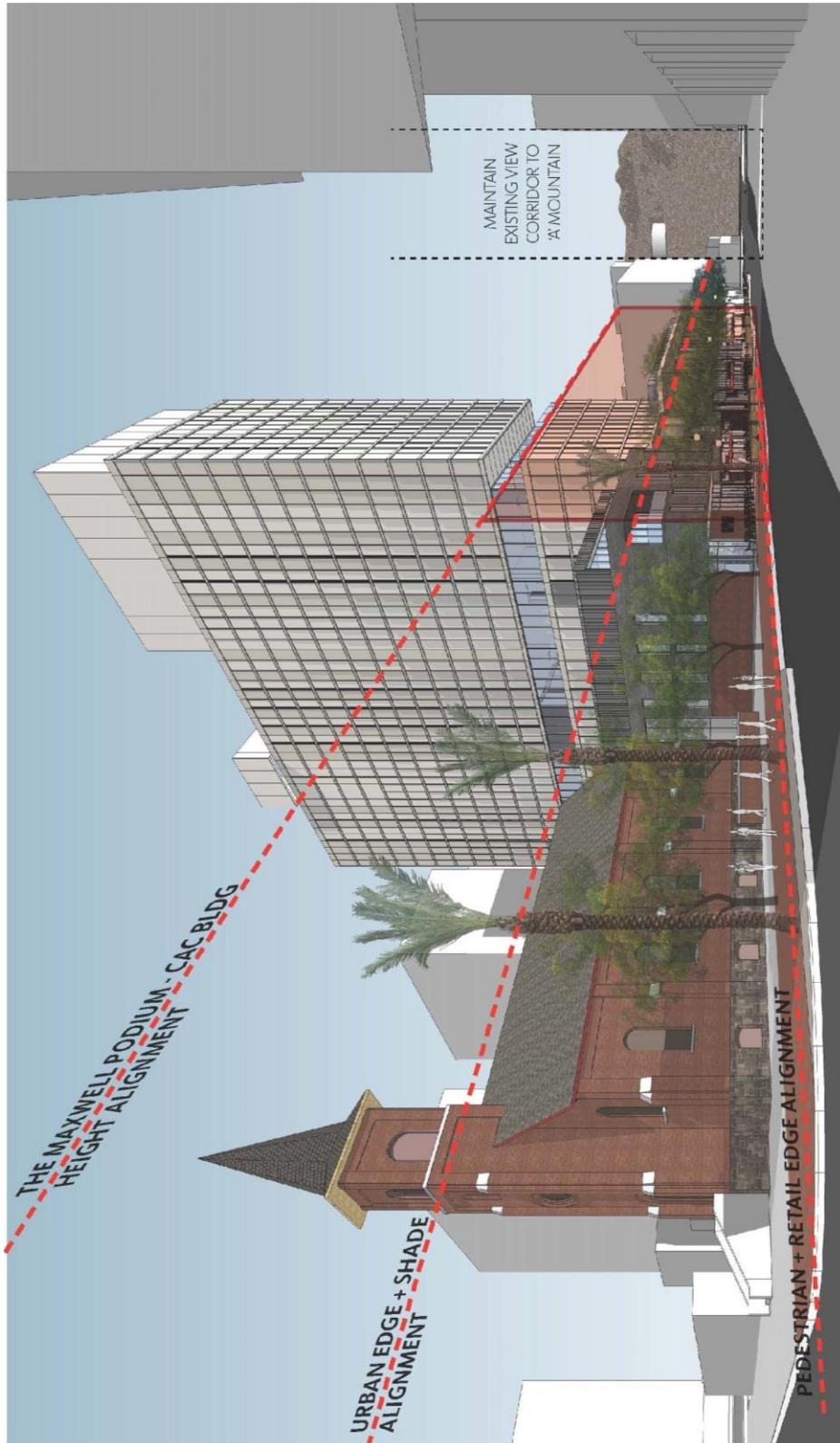
Over the course of the last decade, College Avenue has become a major pedestrian entry to ASU, anchored by the Stadium and Tempe Transit Center on the north, College Avenue becomes Cady Mall as it enters the campus south of University Drive. It is likely that in time, the ever increasing pedestrian traffic along this route will make it an impractical corridor for vehicle traffic except busses and service vehicles. As such, careful attention has been paid to orchestrating the design of this environment. The same architectural team that ASU used for the design of the CAC building and streetscape, Gensler, is the architect for this project. In that way the project is able to ensure that the massing, building lines and site lines would complement those established by ASU's CAC building and the Historic Old Saint Mary's for College Avenue.

The neighboring ASU Foundation Building (a.k.a. Fulton Center) was massed and placed to enclose the forecourt to Old Main (south of University Drive). ASU's College Avenue Commons (CAC) building was positioned and massed to hold the corner and provide a shaded, active streetscape along a portion of College Avenue designed to be closed on game days. The placement and massing of both of these buildings obstruct the major views of A Mountain from the campus and University Drive. As such, the placement of the proposed building masses are carefully aligned to match the building line and overhangs of the CAC building on the lower three (3) levels, in that way ensuring that no more of the open corridor to A Mountain at the north end of College Avenue is obstructed, see Exhibit K – College Avenue Building and Site Lines. We were asked by City Staff to step the upper levels of the building back and to accommodate their request, we have not encroached into the right of way along College Avenue above the Sky Park.



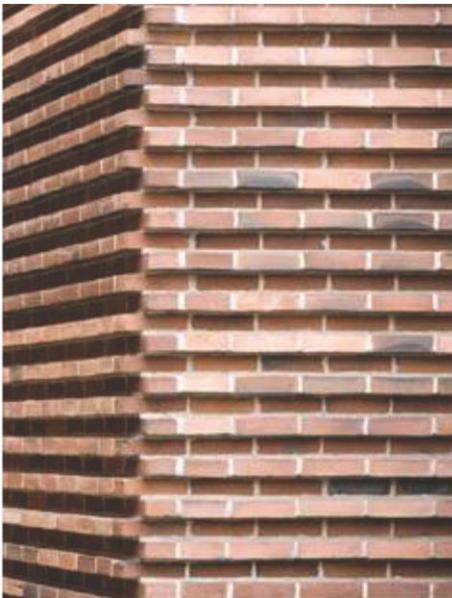


EXHIBIT K
COLLEGE AVENUE BUILDING AND SITE LINES

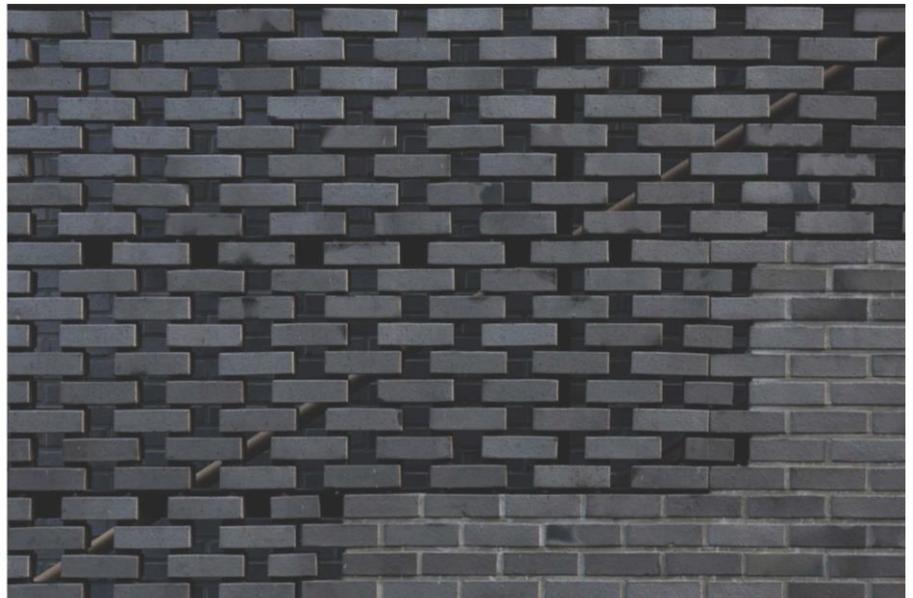


MATERIALS

This will be a glass, steel and concrete structure as requested by the City for buildings in this part of the City. Along College Avenue and 7th Street, the buildings, on the ground level, continue the theme set by the old church and the Foundation Building. The first floor façade is red brick undulated to create shade patterns. This pattern complements the darker tones and rougher texture of the stone work on the Old Church and completes the College Avenue streetscape. On the second and third levels of the Newman Offices the façade is a mix of red and charcoal brick in typical and decorative patterns. This complements the upper levels of the old church and begins to transition to the residential building. The second and third levels of south tower of the Maxwell on the College Avenue frontage are in a charcoal color brick with varying decorative patterns. On the north tower the charcoal brick continues to the fourth level along the 7th Street frontage, see Artist Renderings above. This gray tone complements that of the adjacent ASU College Avenue Commons building, see Exhibit L - College Avenue Enlarged Partial Elevation. Above the brick façade on the Newman Offices and on both towers, the buildings are clad in translucent and opaque glass panels. These panels are broken at the floor with horizontal metal panels capping the end of the floor slab. The glass panels are accented by perpendicular metal fins. The fins are sculpted with an organic taper and provide the illusion of movement in the building façade as one walks around the structure. The fins are broken at the 8th level (the community level) to provide accent in the building and to emphasize the street lines created by the existing structures, see Exhibit K - College Avenue Building and Site Lines. The lobbies in glass appear to slice through the heavier base, and provide a key element tying the upper vertical glass and steel expressions to the horizontal brick façade of the base. Collectively, these materials provide a graceful transition from the historic old St. Mary's church to the contemporary College Avenue Commons building. The high quality, long-lasting, low maintenance materials have been selected to improve the sustainability of the building and, much like the materials of the old church, express its longevity.



UNDULATED RED BRICK



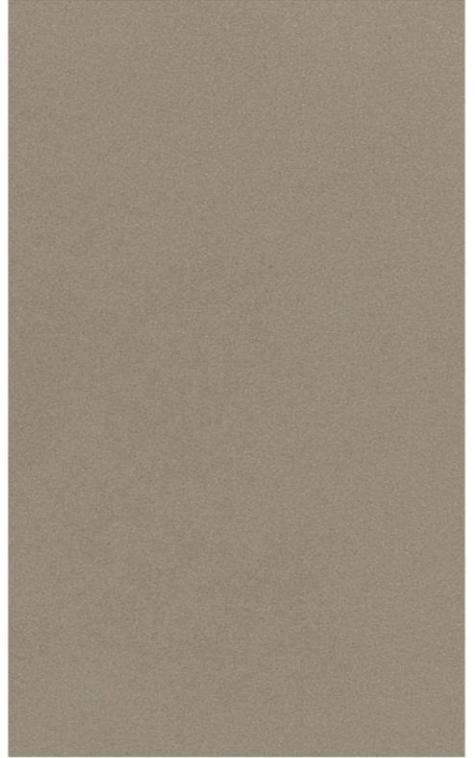
CHARCOAL BRICK IN VARYING DECORATIVE PATTERNS



1 ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM, FIELD COLOR, Centria 9962 XL Silver Gray



2 ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM, ACCENT COLOR, Centria Champagne



9 ALUMINUM SUNSHADE EXTRUSION COLOR, ARCADIA INC, 7594 XE Light Bronze



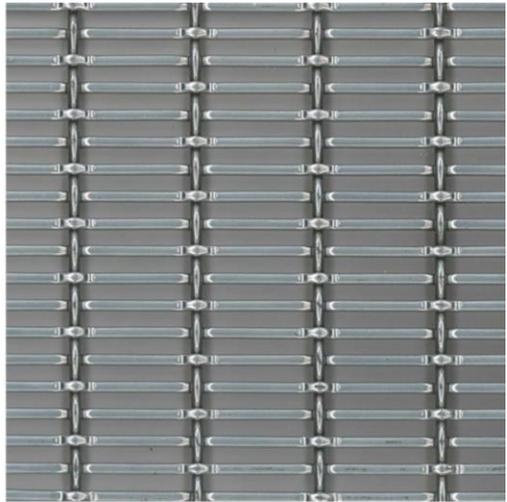
4 MASONRY TEXTURED RELIEF PATTERN Endicott Brick, Color: Manganese Ironspot

5 MASONRY Endicott Brick, Color: Manganese Ironspot

6 MASONRY SCRIM Endicott Brick, Color: Manganese Ironspot



7 MASONRY Endicott Brick, Colors: Red Blend, Medium Ironspot #46



11 PARKING GARAGE - PERFORATED METAL PANEL SYSTEM GKD Metal Fabrics, Ellipse 14, Stainless Steel

PARK PODIUM HARDSCAPE



PARK PODIUM CONCRETE PAVER
Concrete Collaborative - Coal



PARK PODIUM CONCRETE PAVER
Concrete Collaborative - Ivory



PARK PODIUM CONCRETE PAVER
Concrete Collaborative - Mocha



PARK PODIUM DECKING
Mangaris Wood - Natural

SIDEWALK PAVING COLLEGE AVENUE / 7TH STREET



BRICK PAVERS - FIELD
REUSE EXISTING
Color to Match Existing

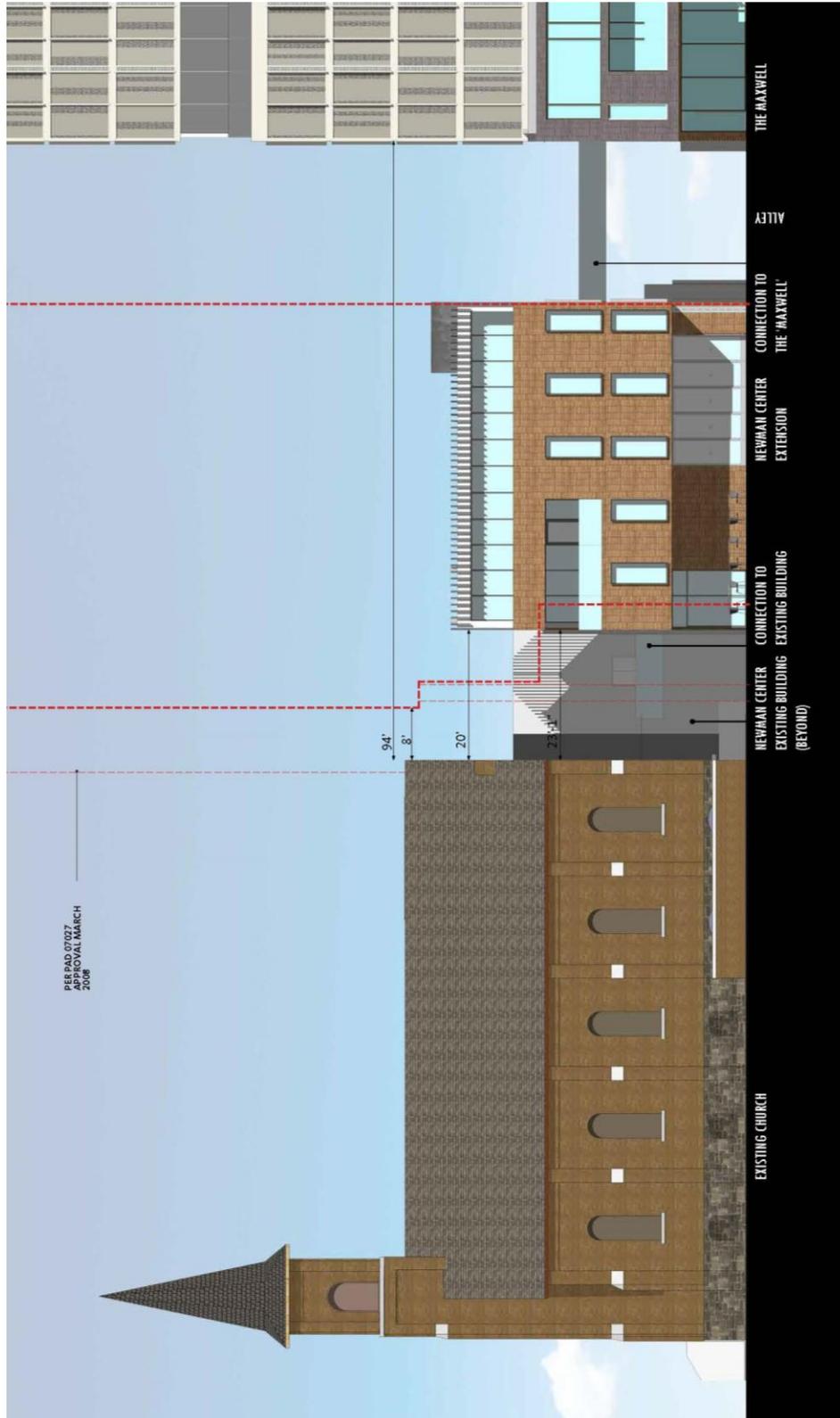


CONCRETE PAVING-ACCENT
Davis Color - Cobblestone



CONCRETE PAVING-ACCENT
Davis Color - Adobe

EXHIBIT L
COLLEGE AVENUE ENLARGED PARTIAL ELEVATION



LANDSCAPE

Much of the mature landscape and paving along College Avenue was originally installed for the visit of Pope John Paul II on September, 14th, 1987. As such, our plans will work to save as much of the landscape and paving as possible. The landscape along 7th Street may be paving dominate and enhanced with planting beds and shade trees. The landscape and hardscape are designed to complete the streetscape character established by the improvements installed by ASU with the CAC Building.

In addition to streetscape landscape at the at the ground level, the existing Newman Center courtyard lawn and planting zones along College Avenue and University Drive complete the open space landscape on the Site. While not officially counted in the Site's open space percentages, additional open space is provided on the 4th level in the Sky Park, see Sky Park section above.



Existing Mature Landscape and Paving Along College Avenue Looking North

LIGHTING

As we are working to create a great dining location along College Avenue, we request to be allowed to match the lighting levels of the restaurants, Postino's and Snooze, existing along College Avenue. This light level which we believe is 3fc, is far more conducive to outdoor dining than brighter light levels. The light levels at the residential entries (the two lobbies), the entry to the office lobby, and the pedestrian alley will meet the ZDC required light levels. We request similar "lesser" levels of illumination be permitted in the outdoor areas of the Sky Park.

VEHICLE AND BICYCLE PARKING

While we are trying to discourage vehicular ownership and use in this very urban area of Tempe by the students who will be living here, we do acknowledge the need to provide parking for all of the commercial uses, all of the residential guests, and some of the residential units that are likely to house couples which may include one member not attending ASU or working in the downtown area. Vehicle parking on Site will be provided on-street and in the on-site garage accessed from 7th Street and the alley, see Exhibit M – Proposed Parking Diagram. The on-street vehicular parking will include nine (9) parallel stalls on 7th Street and two (2) parallel stalls on College Avenue, at the corner with 7th Street. These will continue to be metered by the City's system and will provide fifteen percent (15%) of the parking for the commercial uses north of the alley.

By interviewing other urban student housing projects, we have learned that parking in these structures is often very expensive long term car storage. While students are charged monthly for the right to park in the associated structures, they often find more affordable locations to store vehicles they rarely use because of the surrounding urban context. This often leads to expensive under-utilized parking structures. In an attempt to find the appropriate balance for urban student housing, the upper most levels of the parking garage will be gated and reserved with eighty (80) residential spaces. Per the CC District Parking Code, twenty-nine percent (29%) of the code required vehicular parking for residential units (85 of 294 units) will be fully accommodated by these stalls. These will provide the code required vehicular parking for all of the one (1) bedroom units and the code required vehicular parking for forty percent (40%) of two (2) bedroom units. Vehicular stalls are provided for these units as they are the unit types are most likely to have a partner living in the unit who may not be an ASU student or work outside of the downtown area. To offset the rest of the code required vehicular parking for these unit types (1 & 2 bedroom), we will be providing more than two and a half (2 ½) times the code required bicycle parking for these units. The only three (3) bedroom unit is the rectory and as a live/work unit is covered by the parking reserved for the Newman Center described below. For the convenience of the rectory residents, we will also be providing three (3) times the required bicycle parking for this unit. The four (4) bedroom units are the most dorm like and most likely to be all students because of the unit type (shared baths, limited shared common space, etc.) and being leased by the bed. Instead of providing vehicular parking for these units, we will be providing four (4) times the required bicycle parking.

The ground level, lower level and most of the second level parking will be semi-public paid/metered or reserved parking for the commercial uses, the required residential guest parking, and for the Newman Center and University of Mary's daily use (28 stalls). The number of vehicular parking stalls required for the Newman Center and University of Mary's daily use were determined through extensive interviews with Michele Kilker, Business Manager for the Newman Center. Per their vast experience managing the existing private surface parking lot north of the alley, they are vitally aware of how many spaces are needed for the Newman Center staff and daily

visitors. The Newman facilities rarely (if ever) have all of the staff and additional visitors on-site at the same time. For the past six (6) years, Newman has leased out most of the existing parking spaces on an hourly, daily or semester long basis reserving five (5) to eight (8) for staff uses and keeping three (3) to five (5) available for guests/visitors. Estimating the long term growth of the activities at the center, and incorporating the University of Mary into the site (currently in an adjacent property), Newman anticipated that its needs will grow to twenty (20) daily spaces with eight (8) additional spaces for the University of Mary. In total these twenty-eight (28) spaces will provide all of the required vehicle parking spaces for the commercial uses and eighty-five percent (85%) of the classroom uses south of the alley except the existing office space that will be converted into storage space (in the basement of Historic Old Saint Mary's) and the restaurant space that is separately accommodated in the garage. This vehicular parking will also provide a space for the rectory and its priests as employees and/or a three (3) bedroom residence. Because of the small size of the classrooms being added (conference room sized), and the large percentage of access space in each (walkways, door swings, etc.) they can be vehicular parked more efficiently than the larger more typical classrooms the code was written to accommodate. Approximately eighteen (18) of the stalls will be reserved for Newman Center/University of Mary Staff and ten (10) for their guests/visitors. These will not likely be gated, but may be metered or otherwise limited. On weekends, the Newman Center has made arrangements to park in adjacent facilities and the stalls in the parking garage will be used as handicapped and special access stalls for groups using the church facilities. There will be no vehicular parking south of the alley. All of the code required bicycle parking required for the uses south of the alley will be accommodated on-street in the space between the back of curb and the face of building.

The rest of the vehicular parking on the ground level, lower level and second level will provide 100% of the code required vehicular parking for the residential guests and all of the parking required by code for the commercial uses. It is anticipated that the College Avenue frontage on the first second levels will be retail and restaurant uses with office on the small third level. On the 7th Street façade, the ground floor is anticipated to be retail with some restaurant uses and office uses above. The percentage of each of these commercial uses may be adjusted based on market acceptance of these uses at this location. The percentage of semi-public parking spaces that are for guests and visitors or for employees will also adjust based on the type of use with retail or restaurant being heavy on guests/visitors and office being heavy on reserved for employees. While the commercial uses in the vehicular parking code are allowed to freely adjust, they have separate bicycle parking requirements that must and will also be met in the on-street parking in the area between back of curb and face of building. Because of the dramatically enhanced bike parking being provided for the residents (1 space/bed), additional guest bike parking will not be provided for the residential uses. At least thirty-seven (37) on-street bicycle parking spaces will be provided between the back of curb and face of building to accommodate all of the commercial uses north of the alley.

Because of the close proximity to the ASU campus and the issues it creates with parking in the area, all spaces in the garage will be reserved or metered, limited to support the uses on Site, and therefore not truly public, but called out here as "semi-public" for the commercial and classroom spaces. The vehicular parking stalls permanently reserved for residences are not part of the semi-public spaces. Building management will tow the vehicles of users who have parked in the garage, but are not associate with one of the uses on Property. Upper levels of the parking Garage are gated and reserved parking for residents and potentially the employees of the leased office or commercial space. The gates for the upper levels are within the garage and provide ample stacking length within the garage itself. An empty (striped out) vehicular parking stall will be provided outside the location of the gate so that vehicles that are not allowed to enter can turn around.

In total the project will provide 217 of the code required 516 vehicular parking stalls. As required by City Staff, none of the bicycle parking will be provided on the lower level, and the project in total will be providing 859 bicycle parking spaces, more than twice the code required 363 bike spaces.

RESIDENT AND COMMERCIAL LOADING

Twice a year student residents are loaded into and out of the building through a well-orchestrated "load in" event run by the building management company. Much like it is accommodated for ASU dormitories, this event is staged in the garage with one-way traffic circulating from the alley through to 7th Street. The line of vehicles, with tightly assigned drop off windows, are quickly evacuated into loading bays that are wheeled away to the individual units immediately after un-loading. The un-loading occurs at the base of each tower separately speeding the transition.

Commercial and residential deliveries (packages) are handled through the existing loading zone on College Avenue. Major commercial deliveries to the retail/restaurant uses are accommodated by the commercial loading dock on the in the northwest corner of the site. This dock will be screened from 7th Street with a decorative roll up door, see Exhibit M – Proposed Parking Diagram.

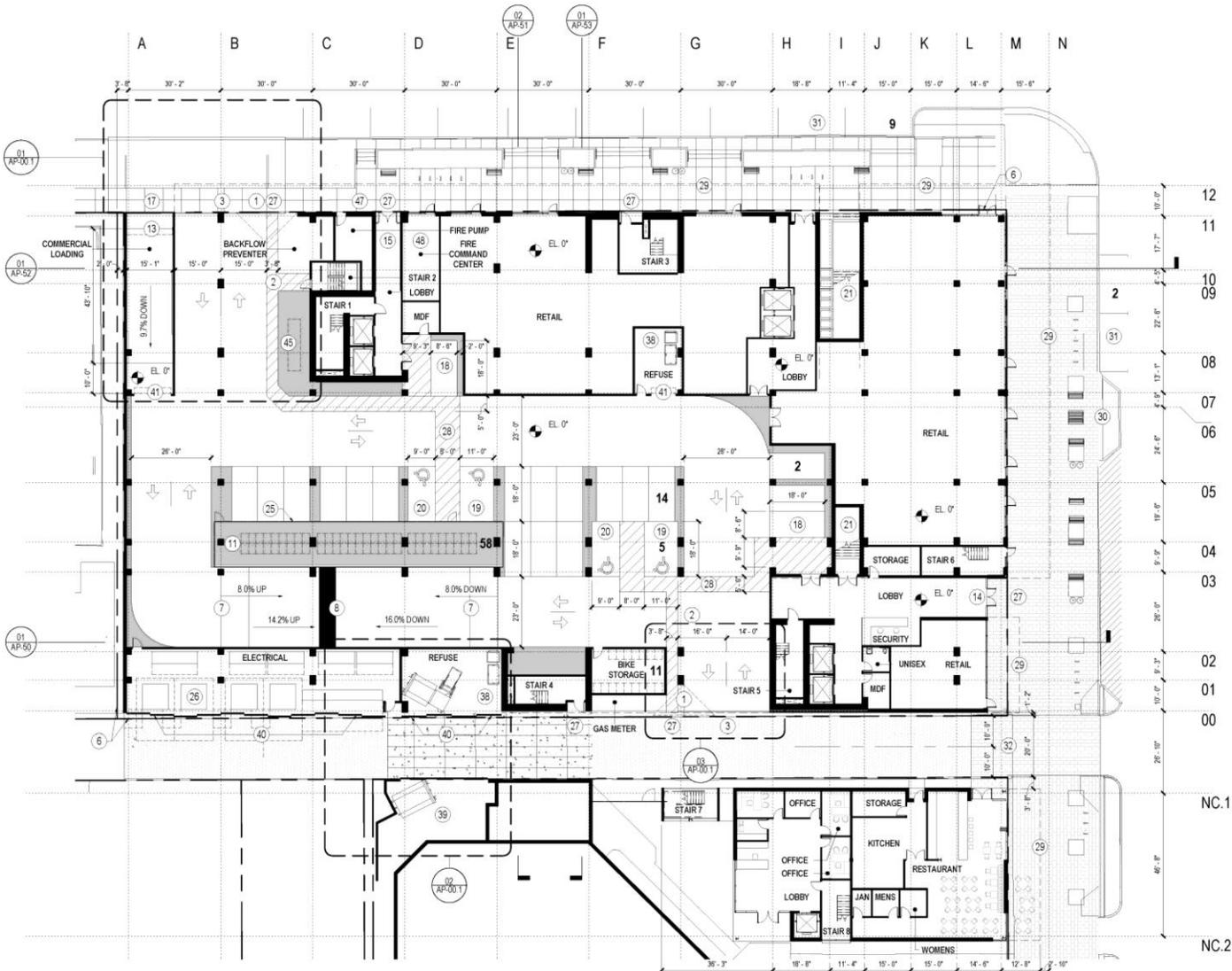
MANAGEMENT AND PROGRAMING

The Newman Center is not interested or skilled in the day-to-day management and operation of student housing, as such, we have partnered with a national property management company with extensive experience in student housing to manage and operate the student housing portions of the project. The management company will incorporate a Newman Center approved code of conduct into its regular operating procedures and work to create the contemplative academic setting we have described herein. The Newman Center and the management company will be jointly responsible for outreach for the leasing of the community.

While events on the existing Newman Center grounds are Catholic based, they are open to the general public and will anchor activities in the residential towers. These events currently include sponsored prayer groups, men's and women's groups, bible study groups, and social events such as dinner on the lawn and dances. In a similar way, while most U Mary classes are limited to registered students, U Mary hosts quarterly guest lecturers and in-depth conversations about the intersection of faith and science. Similar programs will be expanded to support the new residential community on the grounds, and the Newman Center and U Mary will be responsible for programing faith based and social events in the Sky Park and Maxwell multi-purpose spaces to enhance the academic experience of the residents.

Wanting this to be an exclusive place to live, the Newman Center will reach out to other religious organizations affiliated with ASU to offer the opportunity to host students and programing within these faith-based towers.

EXHIBIT M PROPOSED PARKING DIAGRAM



CITY OF TEMPE PAD APPROVAL CRITERIA

The development fulfills certain goals and objectives in the General Plan, and the principles and guidelines of other area policy plans. Performance considerations are established to fulfill those objectives.

As described below, the Newman Center's Maxwell project fulfills many of the goals and objectives of the General Plan 2040 (Dec 2013) as well as the principles and guidelines of the Community Design Principles, Downtown / Mill Avenue District and Vicinity (Apr 2006), Mill + Lake District Streetscape Principles + Guidelines (Feb 2011) and the Tempe Urban Open Space Plan (Dec 2007).

GENERAL PLAN 2040 - Goals and Objectives

MAJOR THEMES OF GENERAL PLAN 2040

Develop as a leader in "Urban Living," Expand pockets of urban activity centers or "hubs," and enhance connections for pedestrian, bike and transit to produce a "20-minute city"

The Newman Center's Maxwell project will be the first of its kind in urban living - a faith based student housing community in an urban glass, steel and concrete high-rise setting rather than the traditional remote, stone monastic one. This will be a unique partnership of four (4) landowners and two religious organizations (Newman Center and U Mary) to create a contemplative faith-based place to grow academically and spiritually at the ASU main campus. It will expand and intensify the existing urban activity center Downtown and bring additional pedestrian life to the College Avenue corridor. Finally, the project will enhance connections for pedestrians and bikes with the addition of retail activity, a shaded walkway, and sidewalk dining along 7th Street to connect the uses of College Avenue to those at Forest Avenue and 7th Street. It will maintain the great pedestrian and bike connections along College Avenue and enhance them by opening up the retail uses to the street and adding additional pedestrian activity to support the retail uses in the off hours.

LAND USE ELEMENT

Goal: Foster quality development through land use that provides sustainable growth and enhances the quality of life where people live, learn, work and play

Objectives:

- LU1 Establish development of multiple hubs with higher density cores serving the surrounding neighborhoods as its mixed-use urban activity center***
- LU2 Promote land use patterns that encourage long-term sustainability***
- LU3 Seek balance and compatibility of new land use development with established residential neighborhoods***
- LU4 Encourage a balanced community with a diversity of uses and employment opportunities***
- LU5 Provide land use and transportation integrated planning, design and development that reinforces and enhances the character of the entire community***
- LU6 Promote compact, efficient infill development***
- LU7 Encourage preservation of significant historic and archeological resources***
- LU8 Develop and implement plans that address particular geographic area needs***
- LU9 Promote neighborhood enhancement and livability***
- LU10 Ensure that new development will be consistent with general plan goals and objectives***

LU11 Ensure that public participation standards include ongoing communication and active involvement

This urban, mixed use project is proposed in Tempe's downtown core. By redeveloping the existing mid-century development sites, this project will convert single level retail and a surface parking lot into uses that are more appropriate to the urbanizing portions of Tempe. This new development will support the growing population of ASU and provide some relief for the established residential neighborhoods surrounding the campus.

The proposed uses are only possible because of the site's extensive transportation options at this location - only a few blocks from the Tempe Transit Center offering extensive bus and light-rail connections. The unique site, immediately adjacent to ASU's campus, is perfect because the student housing and ground level commercial uses can be supported by pedestrian and bicycle traffic. These are already heavy uses in the area, and widely supported by the urban streetscape with wide sidewalks, bike lanes, bike parking and extensive pedestrian only mall system on campus.

The character of the proposed development reinforces the recent improvements to College Avenue and 7th Street made by ASU with the College Avenue Commons (CAC) Building. The character also enhances the character of the surrounding community by complementing the design, form and intensity of the nearby 7S project and adjacent project on the southeast corner of 7th Street and Forest Avenue. Like these other adjacent projects, the proposed development will be compact, efficient infill development. It will take the existing approved student housing element and spread it across a larger site, add to the residential component and enhance ground level commercial to activate the streetscapes of College Avenue and 7th Street. The retail component will be similar in scale to all of the other contemporary projects providing activity along the entire frontage. The residential component will be less dense than previously approved, and not as dense as some projects in the immediate area, but generally as dense as recent residential projects in the area.

The proposed project will continue to protect the historic Old Saint Mary's church on the site (1903). The new design will place a greater emphasis on the old church physically with dominate views from the 8th level community rooms in the South Tower. The proposed project will allow more space around the historic landmark than the current entitlement does by setting the residential towers further back from University Drive, reducing the height of structures immediately adjacent to the old church, see Exhibit H – Proposed College Avenue Composite Elevation. The proposed design will also provide more of a transition from the design elements of the historic structure to the very contemporary design elements of the CAC building. The building immediately adjacent to the old church will be predominately red brick with window openings and massing to complement the historic structure. This transitions to charcoal brick as we move north along College Avenue increasing the amounts of gray in the structure as well as the amounts of glass and metal panels.

The proposed project addresses the particular design needs of College Avenue, an important developing corridor in the downtown Tempe core. This corridor is distinctly different than Mill Avenue or University Drive. It is on a daily basis the most heavily used pedestrian and bicycle route in the city. It connects the Tempe Transit Center with ASU's campus. It is the site of ASU's "welcome to campus" activities including the starting point for all campus tours. It is often the first look of campus when the outside business world engages ASU through the Foundation Building. Finally, it is the connection between campus and the stadium for events, and is often partially closed to traffic for large parties. As such, it is important that this project continue to provide the active

ground level with retail and restaurant uses, outdoor dining in a shaded environment, wide sidewalks, bike lanes and abundant bike parking. It was also important that elements such as the lobby or social spaces interact with the streetscape, but take up as little frontage on the ground level as possible. The towers align east-west, to minimize their frontage on College Avenue so they do not dominate the street, and enhance the street lines already established by the recent urban additions, see Exhibit K - College Avenue Building and Site Lines & the College Avenue Streetscape and Massing section above. Finally, it was important that the pedestrian link of the existing alley remain, so this project proposes undergrounding the adjacent portion of the power and communication lines as well as repaving the alley for the length of the project to ensure that it will remain a vital pedestrian link.

The proposed project is consistent with the general plan goals and objectives and has worked to ensure that the public has been engaged throughout the process. Meetings were held on site with the congregation, the general public and key community leaders. The development team has met individual with many community leaders to share the goals and aspirations of the project and the Newman Center / U Mary team, soliciting their feedback and integrating their ideas into the proposed development.

COMMUNITY DESIGN ELEMENT

Community Design Goal: Promote design and development standards that improve the community's visual quality, urban form, and functionality to enhance the quality of life for future generations.

Objectives:

- CD1 Create recognizable and usable "places"***
- CD2 Provide focal points***
- CD3 Achieve diverse continuity***
- CD4 Encourage and enhance pedestrian movement***
- CD5 Respond to climactic factors and human comfort***
- CD6 Provide opportunities for interaction and observation***
- CD7 Encourage mixed-use designs***
- CD8 Encourage architecture that will withstand changes in style and economy, enabling adaptive re-uses in the future***
- CD9 Promote sustainable design concepts***
- CD10 Maintain or reduce lighting impacts on night skies***
- CD11 Recognize and celebrate geographic distinctions in architectural character***
- CD12 Utilize the built environment to promote a healthy community and encourage active lifestyles***

Accessibility Goal: Instill solutions for community needs through universal designs that provide universal access and benefit through accessible public and private facilities, services and programs

Objectives:

- AE1 Create adaptive environments capable of meeting current and future needs of the community***
- AE2 Utilize universal design***
- AE3 Where possible, create multi-user access***
- AE4 Promote ergonomic, human-scaled environments***

The site plan for the proposed development utilizes the design principles of connections, permeability and transparency. The vital connections for pedestrians and bicycles along College Avenue and 7th Street corridors are protected and enhanced with the addition of more transparent and engaging retail/restaurant uses along the ground level. These ground level uses will also create a more permeable ground level with users along the street drawn into the outdoor dining and indoor spaces. The site plan also protects the increasing pedestrian activity along the alley corridor. Along the site, power and communication lines will be undergrounded and the alley surface repaved. Landscape along College Avenue will keep its historic paving and mature shade tree character, keeping many of the trees installed for the Pope's visit in the 1987. These oak and pistache trees are consistent with overall streetscape character and accented by the date palms. Along 7th Street, a new landscape and enhanced sidewalk will be installed to complement that established by ASU's CAC building to the north and the 7S project to the east.

The proposed site plan works to further enhance the historic icon on the property – Historic Old Saint Mary's Church. The plan moves the existing entitlement for a student housing tower further north (85' +/-) to provide more space around the historic church. The lobbies for each of the residential towers also provide focal points in the project, breaking the brick façade of the lower levels with a light, glass façade connecting the towers above to the ground level. With these focal points clearly established, the mass of the building provides strong horizontals on the lower level of the building inviting the eye to move down the length of the activated streetscape. On the upper levels of the building, the eye is drawn further upward with strong vertical expressions in floor to ceiling glass and the vertical fins that stretch the length of the building. To avoid monotony however, these fins are broken at the 8th level to provide accent at the community level, and further complement the lines of the streetscape established by ASU's CAC building, see Exhibit K - College Avenue Building and Site Lines. The site plan assists this movement of the eye by offsetting the north and south tower to provide interest in the streetscape and changing vantage points as the silhouette of the two towers change based on the viewer's vantage point. This offset position also encourages the eye to look upward into the space created between the two towers, the Sky Park on the fourth level and the bridge between the buildings on the eighth level.

The architectural elements relate to the existing built context. On the south side, the Historic Old Saint Mary's provides the inspiration with its tall vertical windows, dark and rough stone base and red brick above. Just north of the site, ASU's CAC building is a contemporary counterpart with sandstone ground level slender windows and horizontal metal panels above in variations of gray. The proposed project transitions between these two extremes, with the southernmost portions strongly influenced by the old church with its rough textured first level and red brick above with vertical window expressions. North of the alley the brick work above the first level is primarily charcoal to complement the colors and texture of the CAC building with horizontal metal panels and vertical glass expressions above.

The proposed project is designed to encourage people to walk and bike. This starts with its unparalleled location at the edge of the ASU campus and downtown Tempe – easy walking distance to all of the best Tempe has to offer including Tempe Town Lake, Papago Park, A Mountain and Tempe's Transit Center with its extensive connections to the greater region. The project minimizes automotive parking in favor of bike parking – providing one bike parking spot per bed. This is enhanced by the shade trees along College Avenue and 7th Street and the cantilever of the buildings over the ground level, pedestrian realm further enhancing the shaded pedestrian routes.

The orientation of the buildings responds to the local climatic factors, minimizing the western exposures and protecting the southern exposures with the regularly spaced vertical fins. The orientation also creates shaded environments along the north and east faces of the proposed project which enhance the human comfort in the retail/restaurant spaces at the ground level.

On the upper levels of the buildings, multi-purpose and office uses look over College Avenue, providing a natural observation and surveillance (eyes-on-the-street). Over 7th Street the same is provided with residential uses just above the ground level commercial spaces.

The architecture is designed to be simple and elegant, able to withstand changes in style. The building construction type and floor plan layout allow a great deal of flexibility for the buildings to adapt to varying economic conditions. In addition to student housing, the building type can easily be retrofitted to accommodate hospitality, residential and office uses supported by denser automated parking and off-site parking agreements.

The proposed development has been designed to obtain LEED certification and in addition to the environmentally appropriate orientation and facades, the building will collect rain water for landscape irrigation. The Sky Park will be watered with this through a water conserving drip irrigation system. The building design will also incorporate LED lighting throughout the structures and minimize water amenities to those elements that could not be easily provided in other ways (i.e. pool).

The project is requesting an exception to the City of Tempe S.E.P.T.D. lighting requirements to provide light levels that match those currently found in ASU's new buildings and rehabilitated old buildings along College Avenue. These lower light levels provide a safe, comfortable nighttime illumination levels that encourage outdoor use while protecting the "Dark Sky."

The proposed housing, office, retail, restaurant and educational spaces will all meet American with Disabilities Act requirements for accessibility and include readily adaptable elements for future accessibility. This development will replace older spaces that do not meet these requirements and will provide the accessible element for some of the existing structures on the existing Newman Center site.

The proposed development meets the Community Design Element's goal by improving the community's visual quality, urban form, and functionality and enhancing the quality of life for future generations.

HISTORIC PRESERVATION ELEMENT

Goal: Enhance community character and heritage through the identification and preservation of significant sites, properties and districts

Objectives:

- HP1 Identify, preserve and protect significant historic properties and archaeological sites**
- HP2 Identify those districts for designation and preservation**
- HP3 Foster economic vitality through preservation and adaptive rehabilitation of historic properties that contribute to the character of the community**
- HP4 Integrate historic preservation planning with boards, commissions, neighborhood and redevelopment planning efforts to protect and enhance community heritage and complement city redevelopment and revitalization efforts**
- HP5 Preserve historically significant character of single-family homes that use repair or rehabilitation programs**

Historic Old Saint Mary's (1903) (survey number HPS-235) is one of two churches on the City of Tempe's List of Historic Properties and Districts (Table 7) - First Congregational Church is the other 45 years later in 1948. The old church was a significant part of early Tempe, built 32 years after the C.T. Hayden House and 14 years after Main Building, Tempe Normal School. It is noteworthy that it also built 28 years before the Tempe (Old Mill Avenue) Bridge.

"St. Mary's Church is significant as a prominent landmark in Tempe. This building was constructed to replace the earlier adobe chapel which served the Catholic community since 1881. The project was stimulated by Severinus Westhoff, O.F.M., a German immigrant who had come to the Tempe chapel in 1895, and who had started missions in both Scottsdale and Guadalupe. With the help of volunteer labor from both Mexican-American and Anglo community, the church was dedicated in 1903. Among the prominent citizens in Tempe who were originally involved in the church building project were John Curry, J. J. Hodnett, Winchester Miller, and J. T. Priest. The church was granted parish status in 1932, and was transferred to the Newman Club in 1962." – City of Tempe website – St. Mary's Church

This historic landmark has been protected and restored with the help of the larger community and remains in regular operation today as the Newman Center's offices, meeting space, and on upper level, the historic sanctuary which is still in use. Recent repair and replacement of the church roof made it possible to once again ring the historic church bell. The church is used regularly for religious ceremonies and particularly for weddings that spill out after onto the steps and small plaza at the corner along University Drive.

With our first phase of modern development, replacing the 1962 social hall with a new, larger sanctuary and social hall, the Historic Preservation Commission asked us to work to protect distant views of the historic steeple along University Drive. As such, the social hall and sanctuary components of the 2008 approved plans were flipped on site, so that the lower elements would be placed along University Drive, protecting the distant views. These components of the grounds were completed in 2012 and are in regular use today.

This historic building remains a focal point of the new Newman Center grounds and the proposal moves the student housing towers further north (85' +/-) to provide more visual openness around the landmark than previously approved plans did. The new design will place a greater emphasis on the old church physically with dominate views from the 8th level community rooms in the South Tower. The proposed development will also provide a greater level of architectural transition between the historic structure and the very contemporary design of ASU's CAC building north of it along College Avenue. The building immediately adjacent to the old church will be predominately red brick with window openings and massing to complement the historic structure. This transitions to charcoal brick as we move north along College Avenue increasing the amounts of gray in the structure as well as the amounts of glass and metal panels.

Listed in the National Register of Historic Places since January 30, 1978, the Newman Center has worked for years with Tempe's Historic Preservation Office preserving and maintaining the historic structure. It has been a strong relationship and in addition to its religious duties, the old church has been a part of many historic walking tours in Tempe. As part of this project's approval process, we will be seeking the recommendation of the Historic Preservation Commission.



NEIGHBORHOOD PRESERVATION AND REVITALIZATION ELEMENT

Goal 1: Strengthen community by encouraging residents to engage in their neighborhoods

Objectives:

NP1 Provide a participatory planning process and programs that engage neighborhoods

NP2 Continue to educate and involve the public and neighborhoods in city processes

Goal 2: Enhance neighborhoods with community-inspired solutions, ultimately serving to improve the quality of life

Objectives:

NP3 Promote neighborhood maintenance and enhancement

NP4 Promote a healthy and safe neighborhood environment

NP5 Attain the best possible neighborhood maintenance and appearance through collaboration between property owners and the City

The project team has submitted a required Public Involvement Plan and has held public meetings (one required) to engage residents in the planning process as well as hosting meetings with many of the area's key community leaders. We continue to reach out to members of the community to keep them involved in the process.

The residential portion of the project will be managed by a national professional residential housing management company. They will ensure that the buildings will be maintain to high quality standards.

The Newman Center and U Mary work to create community leaders and strive to engage ASU students in larger social issues - homelessness, feeding the hungry, and social justice. This work will extend to the faith-based residents of the student housing components of the project. It is through these efforts that students will not only live on campus but become active, engaged, young members of the larger Tempe community.

REDEVELOPMENT ELEMENT

Goal: Sustain or maximize the efficiency of land uses within areas of stagnation or decline by promoting the greatest economic, social and cultural potential

Objectives:

RED1 Encourage reinvestment, rehabilitation, redevelopment or reuse

RED2 Prevent and eliminate slum and blight

RED3 Stimulate private investment

RED4 Attract new development that adds to urban livability

RED5 Ensure the provision of adequate infrastructure

The proposed project is within the University / Hayden Butte Redevelopment Area. The proposed development will utilize an existing empty lot that originally was the site of a surface parking lot north of the old church. The project will also develop an existing surface parking lot and single story commercial building and surface parking lot along College Avenue. This project will be an investment of more than \$65 million dollar of private investment,

enhancing the downtown area with quality development. The residents in this tower will be paying rental rates at the top of the market and will likely enhance the economic viability of supporting retail and restaurant uses in the immediate area as car ownership will be discouraged. This will be a captured population that will rely on services provided in the downtown or easily accessible by transit. These limitations will encourage and support development of these types of services in these urban areas rather than ones easily accessible by automotive routes only. The site is adjacent to a major arterial street (University Drive), a few blocks from the City of Tempe Transit Center with extensive bus and light rail service and is along the College Avenue corridor offering a wide variety of restaurants open to the public along the City's most heavily used pedestrian route. The site is well supported by existing infrastructure.

HOUSING ELEMENT

Goal: Provide diverse housing opportunities for current and future residents, for all income levels and household types, with specific focus on providing affordable housing to help those in greatest need

Objectives:

- H1 Encourage mixed-income housing developments and neighborhoods***
- H2 Facilitate property reinvestment to maintain the condition and value of existing housing***
- H3 Support housing development that meets the needs of the disabled, those with special needs, older adults and those aging in place***
- H4 Increase the quality of owner-occupied housing through housing rehabilitation assistance to low and moderate income households***
- H5 Increase affordable owner-occupied housing options***
- H6 Increase affordable rental housing and rehabilitation of existing rental housing***
- H7 Support affordable housing initiatives and work with other public and private non-profit agencies through site acquisition and development activities***
- H8 Create opportunities for low- and very-low-income households to achieve permanent housing***
- H9 Support housing that allows for the greatest level of self-sufficiency, dignity and independence***
- H10 Encourage development of needed housing in close proximity to transit, employment and services***
- H11 Coordinate a collaborative process to reduce the number of people experiencing homelessness locally and regionally***

While the proposed student housing project will not be affordable, it will meet the needs of the disabled and increase (all be it, very slightly) the availability of for sale single family homes in the neighborhoods surrounding ASU by providing student housing for the growing population of ASU. If the proposed project was not developed, an additional 798 beds, that student population would likely rent in the surrounding neighborhoods as all other student housing projects have had little difficulty leasing quickly. The proposed project will allow for a high level of self-sufficiency as it is surrounded by regularly required household supporting services such as the Tempe Market on University Drive and the restaurants of College and Mill Avenues. The proposed project also provides many desired household services such as in-unit clothes washer and dryers, pool and fitness amenities. This proposed housing will be in close proximity to the Tempe Transit Center providing extensive bus and light-rail connections and immediate proximity to the classes of ASU and U Mary.

ECONOMIC DEVELOPMENT ELEMENT

Goal: Stimulate a sustainable, diversified and vibrant economy and job force

Objectives:

- ED1 Sustain a business climate that fosters private business investment**
- ED2 Develop an increased tax base**
- ED3 Promote a sustained improvement in the standard of living and quality of life for all residents**
- ED4 Remain flexible in a constantly changing economy**
- ED5 Attract businesses and employers that provide jobs paying wages at or above the regional average**
- ED6 Maintain and attract a highly trained workforce**

While not the typical economic development project, the Newman Center's Maxwell student housing project expands the Newman Center's and U Mary's ability to train and nurture community and business leaders. These facilities will help attract students considering Catholic university education elsewhere in the country. The ability to combine U Mary's degree in theology with ASU's degree in engineering is a unique opportunity and these facilities will provide the housing component. While they will not be the typical limestone and stained glass residential quads found in the east, they will provide the contemporary, urban counterpart – the best the West has to offer. Such proposed facilities will attract students who will become the highly trained workforce modern employers are seeking. The strong connection to place enhanced by the local religious organizations, their clubs, facilities and alumni will all work to anchor this workforce in this place.

GROWTH AREA ELEMENT RIO SALADO GROWTH AREA

Project is not included in this growth area.

RAIL CORRIDOR GROWTH AREA

Goal 1: Attract mixed use development along the rail corridor and create a dynamic and eclectic urban environment to maximize public investment

Objectives:

- RC1 Focus mixed use development near rail investments to support reduced transportation costs for residents and, in turn, produce increased transit ridership**
- RC2 Create a walkable community enhanced by rail transit**
- RC3 Encourage land use and development that creates a sense of community and place**
- RC4 Preserve historic structures and buildings of significance**

Goal 2: Support development of City and Valley-based employment centers in the rail corridor

Objectives:

- RC5 Revitalize underutilized areas for employment**
- RC6 Maintain high design standards in landscape and building treatments**
- RC7 Ensure improved access to jobs for households of all incomes**

Goal 3: Continue investment in improvements to make the Downtown a financial, civic, cultural and professional hub of the region

Objectives:

- RC8 Sustain the Mill Avenue District as a regional destination**
- RC9 Create a balance of residential, commercial, recreational and educational uses**
- RC10 Create a unique urban environment and authentic image**
- RC11 Encourage investment that builds a strong sense of community**

Goal 4: Encourage reinvestment and establish compact, desirable and walkable urban neighborhoods

Objectives:

- RC12 Continue investment consistent with the public investment in transit and reflective of transit-oriented design and development**
- RC13 Enhance area quality of life for existing and future residents**
- RC14 Promote desirable reuse of land**
- RC15 Stabilize and improve the Apache Boulevard area**
- RC16 Balance density and open space**

The proposed mixed use project is near the major transportation investments at the Tempe Transit Center. The project is designed to discourage vehicular use and encourage pedestrian and bicycle use. This combination will likely reduce transportation costs and increase transit ridership. The development of the site will enhance the walkable environment along College Avenue, one of the most heavily used pedestrian and bike routes in the City. This project will continue to provide the active ground level with retail and restaurant uses, outdoor dining in a shaded environment, wide sidewalks, bike lanes and abundant bike parking. The proposal will add many of these elements to the 7th Street streetscape as well. The proposed development will complete the sense of place being created along College Avenue and preserve the historic Old Saint Mary's Church in place. The proposed development will support the Rail Corridor Growth Area goal with mixed used development along the rail corridor that enhances the dynamic and eclectic urban environment to maximize the public's investment.

The proposed development also supports the Rail Corridor Growth Area goal to make the Downtown a cultural hub of the region. The project will not only provide additional office and classroom space for the Newman Center and their on-going outreach to the students of ASU but also additional office and classroom space for U Mary, Arizona's first Catholic University and student housing to help both attract students and nurture the development of community and business leaders. It is the natural extension of the cultural hub established on the site with the construction of the historic (1903) old Saint Mary's Church.

The proposed development also fulfills the Rail Corridor Growth Area goal to encourage reinvestment and establish compact, desirable and walkable urban neighborhoods. The project will utilize an existing empty lot that originally was the site of a surface parking lot north of the old church. The project will also develop an

existing surface parking lot and single story commercial building and surface parking lot along College Avenue. This project will be an investment of more than \$65 million dollar of private investment, enhancing the downtown area with quality development. As car ownership will be discouraged, the residents will rely on services provided in the downtown or easily accessible by transit. These limitations will encourage and support development of these types of services in these urban areas rather than ones easily accessible by automotive routes only. This compact development pattern will naturally create and enhance the desirable and walkable urban neighborhood downtown.

ARIZONA STATE UNIVERSITY GROWTH AREA

Goal: Collaborate on development, infrastructure capacity and land use issues that are consistent with Tempe's and ASU's long-term needs, and embed ASU in the community through increased campus and community relations and public participation

Objectives:

ASU1 Engage new efforts toward sustainable growth and development

ASU2 Promote public health, safety and welfare on and around the campus

ASU3 Preserve and celebrate historic structures and buildings of significance on campus

ASU4 Provide diverse and compatible housing and academic development, complementary to the neighborhoods

ASU5 Embed the Arizona State University campus within the City

ASU6 Promote Tempe campus regionally as educational hub of the Valley

ASU7 Promote campus and community interaction

ASU8 Create an international identity as the model for University-Community relations

The proposed development sits in within the Arizona State University Growth Area of the General Plan. As a private development it will proceed through the City of Tempe's rezoning and development plan review public processes. The previously approved student housing development proposed and approved on the Site was supported by ASU as described in their letter to Ryan Levesque dated October 17, 2007. In the letter they describe the project as, "an exciting project on the edge of our campus and a welcome addition to the community."

The project will meet many of this goal's objectives by being sustainably designed, preserving and celebrating the historic old Saint Mary's Church, providing faith-based housing compatible with high quality housing in the downtown area. The proposed project also provides expanded space for U Mary's Tempe campus strengthening downtown Tempe's reputation as the educational hub of the Valley. The Newman Center's involvement with the residents will promote student and community interaction through social justice programs such as feeding the homeless and outreach programs hosting in-depth conversations on the intersection of faith, science, business and the modern world. The proposed development provides an international model for faith based housing and education in the New American University.

ASU STADIUM DISTRICT GROWTH AREA

Project is not included in this growth area.

101/202 INTERCHANGE GROWTH AREA

Project is not included in this growth area.

WARNER & I-10 GROWTH AREA

Project is not included in this growth area.

SOUTH TEMPE TECHNOLOGY CORRIDOR GROWTH AREA

Project is not included in this growth area.

BASELINE & RURAL GROWTH AREA

Project is not included in this growth area.

COST OF DEVELOPMENT

Goal: Ensure funding availability for growth and maintenance of all planned development, both public and private

Objectives:

COD1 Encourage development that does not exceed planned infrastructure or service capacity

COD2 Ensure that land use intensification or redevelopment provide for necessary infrastructure or service capacity

COD3 Maintain fiscal stability for the City of Tempe

COD4 Promote a financially sustainable economy with economic development tools

COD5 Provide opportunities for development that benefits the community

The proposed development will be supported by the existing infrastructure present in downtown Tempe.

CIRCULATION SYSTEM-WIDE

Goal: Develop an effective multi-modal transportation system integrated with sound land use planning, thereby creating safe, efficient and accessible mobility for persons, goods and commerce within the City and region

Objectives:

- T1 Develop a functional relationship between the diverse land uses in Tempe and the transportation system that serves them**
- T2 Accommodate regional travel demands with transit and other modes, as alternatives to street widening, to address capacity needs**

The proposed uses are only possible because of the site's extensive transportation options at this location, only a few blocks from the Tempe Transit Center offering extensive bus and light-rail connections. The proposed development does not include any proposals for street widening, but rather discourages the use of automobiles for travel by design. The location and provisions for enhanced bike and pedestrian transit support the general plan goal to integrate the surrounding land uses with the existing effective multi-modal transportation system.

PEDESTRIAN AND BIKEWAY ELEMENT

Pedestrian Network Goal: Develop safe, comfortable walking environments and pedestrian connections to encourage pedestrian travel

Objectives:

- PN1 Increase awareness that pedestrians are a priority in Tempe, and that pedestrian travel is an important part of the overall transportation system**
- PN2 Provide convenient and safe pedestrian access to destinations to promote neighborhood sustainability**
- PN3 Ensure pedestrian accessibility for all**
- PN4 Increase pedestrian accessibility and enhance the pedestrian environment with engaging and interesting experiences for pedestrians**

Bikeways Goal: Expand and enhance bicycle travel within the City

Objectives:

- B1 Provide safe and convenient access between neighborhoods and schools, parks, shopping, transit, employment, and other destinations**
- B2 Ensure that the circulation network and facilities will accommodate all types and levels of bicyclists**
- B3 Facilitate regional bikeway planning efforts to ensure that Tempe's bikeways connect with those of neighboring communities and that Tempe's system is an integral part of the overall region-wide system**
- B4 Improve the bikeways network**

The unique site, immediately adjacent to ASU's campus, is perfect because the student housing and ground level commercial uses can be supported by pedestrian and bicycle traffic. These are already heavy uses in the area, and widely supported by the urban streetscape with wide sidewalks, bike lanes, bike parking and extensive pedestrian only mall system on campus.

The vital connections for pedestrians and bicycles along College Avenue and 7th Street corridors are protected and enhanced with the addition of more transparent and engaging retail/restaurant uses along the ground level. These ground level uses will also create a more permeable ground level with users along the street drawn into the outdoor dining and indoor spaces. The site plan also protects the increasing pedestrian activity along the alley corridor. Along the Site, power and communication lines will be undergrounded and the alley surface repaved.

TRANSIT ELEMENT

Goal 1: Coordinate and produce efficient, safe, convenient and interconnected transit options to increase ridership

Objectives:

TR1 Increase transit modes and services that support ridership increases and an expanded transit mode share

TR2 Facilitate connections among transportation modes

Goal 2: Support transit that facilitates regional and interregional commute patterns

Objective:

TR3 Expand transit availability to regional and interregional systems

The proposed development will protect and enhance the heavily used pedestrian and bicycle route (College Avenue) between ASU's campus and the Tempe Transit Center.

TRAILWAYS ELEMENT

Goal 1: Encourage redevelopment of the street network that balances the needs for various types of travelers and more fully serves all modes of transportation safely and efficiently

Objectives:

TW1 Retain existing traffic capacity while reducing reliance on the automobile

TW2 Ensure the system integrity is conserved through maintenance and preservation

TW3 Establish guidelines that enhance the land use and transportation connection

TW4 Facilitate safe and efficient movement on arterial and collector streets

Goal 2: Encourage transportation interconnections between street, highway and rail networks that balance and more fully serve all modes of transportation safely and efficiently

Objectives:

TW5 Avoid widening highways as the only solution to traffic congestion

TW6 Plan and encourage beneficial rail uses

The proposed uses are only possible because of the site's extensive transportation options at this location only a few blocks from the Tempe Transit Center offering extensive bus and light-rail connections. The proposed development does not include any proposals for street widening, but rather discourages the use of automobiles for travel. The location and provisions for enhanced bike and pedestrian transit, support the general plan goal to reduce reliance on the automobile and more fully serve all modes of transportation safely and efficiently.

PARKING AND ACCESS MANAGEMENT

Goal: Incorporate parking and access management strategies that influence travel behavior and reduce congestion on busy streets

Objectives:

PAM1 Promote consolidated and shared use parking areas

PAM2 Promote a balanced and sustainable community access strategy

PAM3 Ensure neighborhoods are not adversely impacted by parking issues

PAM4 Integrate urban design principles relative to parking facility design and land use policies with transportation and parking needs

The proposed development discourages the use of automobiles for travel by consolidating and sharing the limited vehicular parking provided on-site and by providing parking for bikes as a viable alternative for automobiles in this pedestrian dominated part of the City. Neighborhoods will not be adversely impacted by this consolidation as the corresponding reduction in vehicular traffic while still accommodating the growth of ASU will be a benefit to the neighborhoods which are generally too far away to be considered an alternative parking location.

AVIATION ELEMENT

Goal: Facilitate compatible land uses, minimize airport over-flight noise impacts, and promote easy access to and between different modes of transportation, within Tempe and the region

Objectives:

A1 Encourage regional approaches to aviation transportation, while recognizing the regional role of Phoenix Sky Harbor International Airport

A2 Encourage continued growth at the reliever airports to disperse airport traffic and cargo

A3 Coordinate with regional and federal aviation authorities on aviation issues

The glass, steel and concrete high-rise type of construction being proposed will by its nature minimize airport over flight noise impacts. The location of the Site just a few blocks from the Tempe Transit Center and its light rail connection to the airport promotes easy access to and from air travel.

CONSERVATION ELEMENT

Goal 1: Increase energy efficiency and renewable energy to sustain economic growth, social equity and environmental preservation

Objectives:

- ER1 Seek clean energy solutions and reduction of greenhouse gasses to protect the environment**
- ER2 Promote programs that increase the use of clean alternative energy and enhance environmental quality**
- ER3 Increase energy efficiency in Tempe's municipal buildings and facilities**

Goal 2: Provide energy efficiency leadership to the community and promote sustainable energy programs

Objectives:

- ER4 Encourage energy and resource conservation as part of all development**
- ER5 Reduce the environmental cost as well as the monetary cost of energy**

The site's location only a few blocks from the Tempe Transit Center offer extensive bus and light-rail connections. This allows the proposed development to discourage the use of greenhouse gas producing automobiles for travel. The location and provisions for enhanced bike and pedestrian transit support the general plan objective to increase the use of clean alternatives and enhance environmental quality. Similarly, the use of LED lighting through the proposed buildings will encourage energy conservation, reducing the environmental as well as monetary cost of energy.

LAND ELEMENT

Land Remediation Goal: Support redevelopment of sites with environmental contamination to achieve the best land uses for the community

Objectives:

- LR1 Recommend land-use actions that promote land preservation, restoration, and efficient use of brownfields**
- LR2 Ensure that the North Indian Bend Wash Superfund site is managed to mitigate impacts**
- LR3 Support redevelopment of sites with environmental contamination to achieve the best land use for the community**

Solid Waste and Recycling Goal: To reduce the amount of trash and hazardous waste generated through an integrated solid waste managed approach

Objectives:

- SWR1 Reduce the amount of solid and hazardous waste sent to landfills**
- SWR2 Reduce hazardous waste impacts on landfills and water supplies**
- SWR3 Reduce municipal solid and hazardous waste**

Wildlife Habitat Management Goal: Manage wildlife habitat to ensure a safe urban environment for the healthy coexistence of humans and wildlife, to the greatest extent possible

Objectives:

- WHM1 Monitor and manage wildlife to balance the benefit of nature within an urban setting***
- WHM2 Facilitate habitat restoration where riparian conditions are present***
- WHM3 Develop methods within urban development to mitigate public health, safety and welfare issues involving wildlife in the most humane and natural means possible***
- WHM4 Coordinate with other organizations and agencies on wildlife issues***

Floodplain Management Goal: Manage flood prone areas to reduce damage to people and property and protect natural floodplain functions

Objectives:

- FM1 Manage floodplains for the welfare of people, property and the natural environment***
- FM2 Encourage a comprehensive approach to floodplain management***

While this is a brown field redevelopment site, there is no know contamination on Site. The plans for the site incorporate facilities for separate landfill and recycling pick up. The facilities are shared across the alley by all of the users of the site, with the recycling collection point south of the alley and the landfill collection point on the north.

The Site has very little urban wildlife at the present time, but with the addition of the Sky Park, we are hoping to attract and maintain a small urban bird population.

ENVIRONMENTAL PLANNING ELEMENT

Air Quality Improvement Goal: Improve regional air quality through regulatory compliance, policies and programs that minimize air pollution

Objectives:

- AQ1 Meet or exceed air quality regulatory standards in Tempe***
- AQ2 Reduce the number of vehicle miles traveled locally and regionally***
- AQ3 Include residents and businesses in the efforts to reduce air pollutants***
- AQ4 Promote pollen sensitive landscape treatment***
- AQ5 Support regional incentives, ordinances and procedures to minimize PM2.5 and PM10***

Noise Reduction Goal: Control noise levels for living, working and learning environments free from nuisance noise that affect comfort, productivity, and the enjoyment of indoor and outdoor environments

Objectives:

- N1 Reduce noise impacts though enforcement of the noise ordinance***

- N2 Promote land use and building design buffers that mitigate noise**
- N3 Promote regional noise mitigation and monitoring regionally to protect Valley-wide quality of life**

Ambient Temperature Reduction Goal: Minimize heat island impacts to maintain a comfortable year-round outdoor environment and reduce energy consumption

Objectives:

- AT1 Consider ambient temperature reduction within development, energy and water resources policies and programs**
- AT2 Evaluate local and regional opportunities to address ambient temperature (heat island) impacts**
- AT3 Participate locally in temperature reduction efforts and promote temperature monitoring and mitigation regionally**

Buildings such as this one being proposed locate people's homes in close proximity to the services they need on a regular basis. This coupled with the request to dramatically reduce the automobile's dominance on Site by providing extensive bicycle facilities further enhance the building's ability to minimize air pollution. This combination of location and vehicle reduction will lead to a reduction in the number of vehicle miles traveled locally and regionally.

Designed as a contemplative place to live while studying at ASU, the proposed building's social spaces on the fourth and eighth level are designed to minimize the ability of hosting large, noisy parties (a.k.a. ragers). The building will be managed by a private management company that will also monitor the noise quality of the project and help to ensure compliance.

Finally, the Sky Park will add a landscaped space to the roof of a parking garage, limiting the heat gain in the space between the towers. On the ground level, shade trees and cantilevered buildings will add shade to the paved urban environment.

The proposed project supports the General Plan's goals of air quality improvement, noise reduction and ambient temperature reduction.

WATER RESOURCES ELEMENT

Water Goal: Ensure the highest level of water quality, source reliability and customer service at the lowest possible cost for utility customers

Objectives:

- W1 Rely on renewable and sustainable water supplies and protect access to those supplies**
- W2 Provide drinking water that meets or exceeds all federal and state water quality standards**
- W3 Continue to provide the highest level of water services at the lowest possible cost**
- W4 Develop and acquire new renewable water supplies**
- W5 Maintain safe yield levels of groundwater use**
- W6 Maximize the direct reuse of reclaimed water for non-potable water uses**

- W7 Use groundwater recharge to store excess CAP water and reclaimed water for future use**
- W8 Maintain an effective water conservation program**
- W9 Integrate land use and water planning for proposed new and redeveloped sites**

Stormwater Goal: Capture, infiltrate, re-use and route stormwater to minimize load or total volume of pollutants carried to receiving waters in order to protect those water bodies for their designated uses

Objectives:

- SWP1 Implement storm water pollution control measures to minimize, to the maximum extent practicable, the discharge of pollutants to the State’s water bodies from Tempe’s storm drain system**
- SWP2 Maintain compliance with Tempe’s AZPDES permit by implementing structural and non-structural control measures to satisfy the terms of the permit**

Wastewater Goal: Safely collect and treat wastewater from residences and businesses using the best available technology and most cost effective means of treatment

Objectives:

- WW1 Utilize the best available technology and most cost effective means of wastewater collection and treatment**
- WW2 Implement programs to ensure compliance with applicable regulations**
- WW3 Keep wastewater utility costs as low as possible for utility customers**
- WW4 Maximize the reuse of reclaimed water for appropriate non-potable water uses**

While this proposed development will have little impact on the goals and objectives of the Water Resources Element of the General Plan, it was reviewed by these departments, meets code requirements for these facilities and will enhance the City water supply moderately by collecting rainwater to use in the landscape via a drip irrigation system. As required, stormwater will be stored on-site in underground containers.

OPEN SPACE ELEMENT

Goal: Provide a variety of natural and landscaped open spaces and parks that serve the diverse and changing needs of an urban community

Objectives:

- OS1 Maintain and enhance existing open space and parks to serve the changing need of the community**
- OS2 Plan and identify opportunities for new open space and parks in the Parks Master Plan**
- OS3 Evaluate the connection between new development and the open space and parks needed to serve that new development**
- OS4 Provide exemplary maintenance of parks and recreation facilities**
- OS5 Utilize Public/Private Partnerships and volunteerism to preserve and enhance open space and park facilities**
- OS6 Create a system of linked open space and parks throughout the City**

While the proposed project will have no public parks, it will dramatically enhance the connection between ASU (dense developed area) and A Mountain (desert open space) via College Avenue. The residents of the Maxwell will have a great urban street network to connect them not only to the desert open space of A Mountain, but also to the Tempe Town Lake urban recreational core and the park and desert open space of Papago Park just beyond.

Separate from the public system, the project will provide an extensive Sky Park, covering nearly a third of the Site north of the alley. This semi-private park will serve the residents as well as the Newman Center who will be providing programming to this park space. While not a setting for outdoor sports, it will host, together with the eight level community space, a wide range of urban fitness activities. Both levels will also provide gathering spaces that open to the outdoors with walkout rooms on the park level and walk out balconies on the community level.

RECREATION ELEMENT

Goal: Promote health, physical fitness, leisure, creativity and entertainment with programs serving a diverse range of abilities and interests

Objectives:

- R1 Consider current community needs and interests rather than traditional services***
- R2 Provide a variety of recreational opportunities that reaches as many citizens as possible***
- R3 Identify mechanisms for funding to ensure the sustainability of programs and facilities***
- R4 Encourage community involvement and volunteerism with recreation programs***
- R5 Work with other agencies to identify and augment recreation opportunities***
- R6 Renovate and renew the recreation facilities***
- R7 Utilize technology to enhance outreach and service delivery to the community***

The urban context of the site will naturally encourage walking and biking as primary methods of transit (rather than automotive that adds not fitness benefit). The Maxwell will include a Sky Park on the fourth level. While not a setting for outdoor sports, it will host, together with the eight level community space, a wide range of urban fitness activities. Both levels will also provide gathering spaces that open to the outdoors with walkout rooms on the park level and walk out balconies on the community level. The eight level will host a variety of fitness activities including space for work out equipment, meditation and yoga.

PUBLIC ART AND CULTURAL AMENITIES ELEMENT

Goal: Enhance and promote Tempe as a diverse, stimulating cultural, library and arts community where cultural amenities inspire and enrich people's lives and experiences

Objectives:

- PACA1 Maintain a strong commitment to advance Tempe as a vibrant and progressive community for cultural and artistic activity***

- PACA 2** *Continue to collaborate with the community partners, neighborhoods, artists, cultural groups, educational institutions and other entities*
- PACA 3** *Enhance the diversity of art, library and cultural amenities, facilities and collections that support, educate and enrich the community*
- PACA 4** *Encourage incorporation of public art into major public and private projects to enhance the city's community character as well as the built environment*
- PACA 5** *Encourage the continuation and expansion of innovative arts, cultural and library programming that further enriches the community*
- PACA 6** *Ensure access to arts, library and cultural amenities to benefit the entire community including residents, businesses, visitors, and tourists*
- PACA 7** *Protect and promote artistic expression and cultural awareness to bring people together to celebrate diverse traditions that strengthen Tempe's sense of community and place*
- PACA 8** *Utilize technologies to provide greater access, build public awareness and encourage participation in arts, cultural and library activities*

The proposed project will protect in place the public art present in the palm tree bus stop on the Newman Center property on the south end of the Site, along University Drive. The buildings will also provide several highly visible locations for the presentation of additional public art.

PUBLIC BUILDINGS AND FACILITIES ELEMENT

"Additionally, schools, colleges and universities, museums, hospitals, county, state and federal buildings provide essential government and community services in Tempe."

(no goal or Objective PB1 provided in the General Plan)

Objectives:

- PB2*** *Design public structures with flexibility for future needs*
- PB3*** *Build structures for sustainable long-term use*
- PB4*** *Promote design excellence while achieving community compatibility*
- PB5*** *Provide for fiscally sound planning, design and construction decision-making of public buildings*
- PB6*** *Practice universal design principles (such as ADA requirements) for maximum comfort and access*
- PB7*** *Promote use of semi-public or private facilities for shared uses that serve the community*

While the proposed project will not include any public buildings or facilities. The University of Mary and Newman Center (church) elements provide essential community services in Tempe. As such, the building type proposed was chosen because of its long durability, its ability to be easily remodeled to accommodate future uses, and its sustainable long term use qualities. The design as described above is intentionally compatible and complementary to the historic 1903 structure on Site as well as the contemporary 2013 structure just north of the Site. As a private financed building, it will be fiscally sound and accommodate ADA requirements.

PUBLIC SERVICES ELEMENT

Public Services Goal: Provide efficient and effective public services to serve current and future community needs

Objectives:

- PS1 Maintain high levels of service to residents, businesses and visitors**
- PS2 Facilitate planning for future service needs**
- PS3 Provide cost efficient means of service delivery**
- PS4 Promote public and private service provision where appropriate**

Other Facilities and Services Goal: Coordinate and jointly plan for the non-city services and facilities to meet the community needs

Objectives:

- PF1 Provide facilities that promote community interaction and build relationships with service providers**
- PF2 Promote use of semi-public or private facilities for shared uses that serve the community, except where a defined service territory is established**
- PF3 Facilitate activities of providers of public utilities to ensure coordinated infrastructure improvements which support technology advancements and required system expansion of enhancements**

Human Service Goal 1: Improve the quality of life for all Tempe residents, with an emphasis on the most in need

Objectives:

- HS1 Establish a comprehensive system of human services that works seamlessly to support residents**
- HS2 Work with residents to determine Tempe's service provision and to guide priorities for services offered by non-profit human service partners**
- HS3 Incorporate services that facilitate senior well-being and aging in place**
- HS4 Integrate land planning and redevelopment efforts with human services located within and throughout the community**

Human Service Goal 2: Support learning and education in Tempe

Objectives:

- E1 Seek academic and social links with learning institutions, their students and educators**
- E2 Encourage and support sharing of facilities**

While the Newman Center has been a provider of Human Services for decades in our community. The proposed development project will have little impact on goals and objectives of the Human Services Element of the General Plan, other than to expand Newman Center's and U Mary's ability to train community leaders who may be instrumental in leading efforts to provide such services in the future. As high-end student housing, the project itself will not likely create any additional need for Human Services.

MUNICIPAL COURT ELEMENT

Goal: Provide the community with an independent judiciary which serves the public through fair and impartial administration of justice

Objectives:

CAJ1 Improve public access to justice through court services, facilities and technology

CAJ2 Enhance transparency, accountability and operational efficiencies through educational efforts and partnerships

The proposed development will have no effect on the goals and objectives of the Municipal Court Element of the General Plan.

SAFETY ELEMENT

Emergency Management Goal: Plan, prepare and coordinate operations to prevent or minimize impact of disasters and ensure appropriate response and recovery operations for large scale emergencies

Objectives:

EM1 Take all appropriate steps to minimize or prevent disasters from occurring

EM2 Maintain a strong disaster response and recovery capability

EM3 Enhance public education for disaster preparation, survival and recovery

EM4 Maintain flexibility to address new issues, respond and change as necessary

EM5 Maintain safe use, storage and disposal of hazardous materials

EM6 Maintain safe routes for public evacuation and emergency responder access to an area during a disaster

Public Safety/Law Enforcement Goal: Enhance and promote the safety of the community and suppress crime

Objectives:

LE1 Fight crime and enhance public safety

LE2 Support and develop a law enforcement organization that serves the community

LE3 Promote community involvement

LE4 Enhance innovation and technology

Fire Operations Goal: Plan and provide for public safety and welfare of the public through preservation of life and protection of property from fire and hazardous materials

Objectives:

FP1 Prevention of fires and other emergencies through an effective fire code development and management program

FP2 Develop a strong cooperative working relationships with all appropriate agencies

- FP3 Respond to emergencies like fire, medical, hazardous materials and rescue calls in a timely, professional and efficient manner to minimize loss of life, property or damage to the environment***
- FP4 Provide a leadership role with teaching and educating residents, children, and visitors how to take care of themselves and neighbors during times of emergency***
- FP5 Establish a highly skilled workforce that is able to address the future demands of public safety and emergency medical services***

While the proposed project has been designed with input from the Police and Fire Departments and reviewed by both departments, the development itself will have little impact on the goals and objectives of the Safety Element of the General Plan.

COMMUNITY DESIGN PRINCIPLES – DOWNTOWN/MILL AVENUE DISTRICT

The Community Design Principles Downtown/Mill Avenue District, identifies four “cornerstone” principles: integration, innovation, articulation and definition as concepts that should be incorporated into every project downtown.

Integration

Community > Area/District > Subdistrict

Newman Center’s Maxwell project keeps and enhances the historic street and alley grid. It does not provide any barriers to historic pedestrian movement through the site, and enhances connections along College Avenue and 7th Street.

Integration

Buildings > Streets/Spaces

The project incorporates sidewalk dining and display areas; balconies at the Sky Park and community levels; overhangs in the lower levels to shade the sidewalk; and a high degree of transparency and permeability with windows and entries all along as much of the 7th Street and College Avenue facades as the City will permit. The staggered placement of the towers, and the massing of the uses in two narrow east west towers allow for perception of an expansive sky canopy from streets. On-street parking is used as a streetscape element, and structured parking is faced with building elements containing other interactive uses.

Integration

Buildings > Function/Environment

Per the detailed description above, forms, details, materials relate to the adjacent historic old church and contemporary CAC building, providing a transition between the two with compatibility at each end. Moving the residential towers north, provided more space and a better relationship with the adjacent designated Historic Property. The proposed project orients the towers in an east-west direction to minimize solar heat gain and includes a Sky Park landscape setting on top of the parking garage to minimize the heat island effect. These efforts are complemented by the tree lined streets which shade the pavement dominated urban infrastructure. The project will enhance the connectivity to the urban open spaces of A Mountain, Tempe Beach Park, and Papago Park beyond through additions to the pedestrian street grid of downtown Tempe.

Innovation

Planning

The design of the Maxwell accommodates the commercial lower levels and student housing in the upper levels while the construction type ensures that the building can have additional anticipated uses if those should prove unsuccessful or unneeded at this location in the future. a. Allow for multiple uses at the outset and anticipate new uses and technology in the future. The site plan respects and enhances the street grid and urban context, yet creatively accommodate access for service and sanitation.

Innovation

Urban Design

The Maxwell is designed to enhance human comfort, interaction and wayfinding with active uses on along the street frontages that engage passers-by. The urban street landscape is hardscape dominated to relate to the existing context and uses. It will continue the established theme set in 1987 for the Pope’s visit and complement

the overall streetscape experience by providing two blocks of more historic “background” to the CAC’s block of contemporary streetscape furniture. Amenities such as seating and sidewalk dining will be found along the length of the street frontages. A final element to enhance the experience is the staggered placement of the towers with the Sky Park in between and the ability to look up into the space from the ground level. This provides a “sense of discovery” to enhance “the journey.”

***Innovation
Architecture***

The buildings of the Maxwell combine traditional materials – brick and glass in new forms to create a timeless transition between the historic and contemporary buildings along College Avenue. The project strives for sustainability by incorporating passive and active strategies, utilizing durable, energy-efficient materials and designing to accommodate a variety of uses and tenants over time.

***Innovation
Visualization***

Please note the sketches, vignettes and models throughout this submission intended to enhance the design process and make the concept “accessible” to a wider audience.

***Definition
District within Community***

The buildings of the Maxwell are different from those around it, yet they relate to the adjacent structures. They are a part of the concentration of tall buildings in the urban core, resulting in an identifiable skyline.

***Definition
Spaces, Places and Streets***

The Maxwell is designed to establish edges and contribute character in the form of “traditional” street fronts with an interactive street “wall” on along the street frontages that engage passers-by. The urban street landscape is hardscape dominated to relate to the existing context and uses. It will continue the established urban accessories set in 1987 for the Pope’s visit and complement the overall streetscape experience by providing two blocks of more historic “background” to the CAC’s block of contemporary streetscape furniture.

The placement of the buildings with lower levels cantilevered over College Avenue, and the upper levels held back to the property line, maintains the existing views up the center of College Avenue to A Mountain. Other views of the mountain from the ground level have been screened by ASU’s CAC and Foundation Buildings, so the proposed buildings will not further impede those views.

***Definition
Buildings***

The entries to the Maxwell are accentuated by long vertical architectural forms. They lightly touch the ground level without interrupting the other activity generating uses along the street frontage. The commercial lower levels are architecturally expressed in horizontal brick forms to keep the eye moving along the street frontage while the residential portions are expressed in glass and steel vertical forms to move the eye skyward.

Articulation

Lower Building Façades [streetfronts]

The lower level building facades are red brick, placed in an undulating pattern to create shade, texture and shadow at a human scale. The ground level open into residential lobbies and commercial storefronts to create a highly permeable pedestrian experience. The street frontage is broken into fairly traditional building opening widths, but contemporary tenants may take more than one bay.

Articulation

Upper Building Façades

The upper towers architecturally strive to exaggerate the vertical elements. From the Sky Park up, the buildings are set back from the cantilevered lower levels shading the street, and rise as two east west forms connected by an eighth level bridge rather than as a massive L shaped building holding the corner. Both towers are set back from the corn so that at this urban open area, views open up with the towers in perspective.

Articulation

Building Tops [skyline]

The building tops for the Maxwell towers are designed to be internally illuminated boxes of perforated steel and translucent glass panels. These will give a playful component and a reason for the eye to look skyward at night.

Articulation

General

The Maxwell towers and Newman Office building use the traditional tripartite form abstracted with contemporary use of traditional materials. The base for both structures is undulating red brick on the ground level and in the Newman Office building this is the full base. In the taller Maxwell towers, the levels above in red and charcoal brick complete the horizontal base. The shafts on the Maxwell are the two glass towers and their strong vertical geometries. On the Newman Office Building the shaft complements the base in brick but adds vertical window elements. For the cap, the Maxwell towers use internally illuminated boxes of perforated steel and translucent glass panels to screen mechanical equipment, while the Newman Office building is capped with vertical expressions similar to those of the shaft of the Maxwell towers.

The Community Design Principles Downtown/Mill Avenue District and its four “cornerstone” principles are clearly incorporated into this project.

MILL & LAKE DISTRICT STREETScape PRINCIPLES + GUIDELINES

The proposed project is in line with the purpose and principles of the document. The Newman Center/The Maxwell on College is designed to enhance the community’s quality of life for future generations by creating a memorable and sustainable sense of place with an aesthetically pleasing theme, safe human scaled pedestrian environment, adequate lighting, shade, and circulation accessible to all as described in this letter. 7th Street is an Internal Connector as described in the Street Types / Character section. College Avenue and University Drive are Town + Gown streets and the proposed Newman Offices and Maxwell student housing will complement the ASU uses along these corridors, providing an enhanced entrance from the Tempe Transit Center to the campus.

URBAN OPEN SPACE PLAN

The Urban Open Space Plan does not specify any specific uses or areas on this block except the alley way which it refers to as a "Pedestrian Back Road." These plans will keep the alley open for such uses and enhance the alley slightly by removing the overhead powerlines (the length of the Property), adding screening for trash and utilizes on Site, and repaving the alley adjacent to the Site.

CITY OF TEMPE PAD APPROVAL CRITERIA (continued)

Standards requested through the PAD overlay district shall take into consideration the location and context for the Site for which the project is proposed.

As expressed above, the standards requested through this PAD overlay district request take into consideration the location in Downtown Tempe at the major pedestrian entrance to ASU. The standards also are appropriate for the context of the Site, transitioning aesthetically from the architecture of the historic Old Saint Mary's to the very contemporary architecture of ASU's CAC building. In its streetscape design, both College Avenue and 7th Street are designed to complement their counterpart on the opposite side of the street.

The Development appropriately mitigates transitional impacts on the immediate surroundings.

The proposed project provides a transition between historic and contemporary architectures on each end of the block completing a key segment of the entry to ASU. It works to mitigate traffic impacts by limiting the number of cars that can park and dramatically increasing the availability of bicycle parking. Finally it provides faith-based student housing to complement the block of student housing ASU has just to the east of the site with towers of similar scale.

As required in Section 6-305 D. Approval Criteria for P.A.D., the proposed project fulfills certain goals and objectives of the General Plan and the principles and guidelines of the Community Design Principles – Downtown/Mill Avenue District, the Mill & Lake District Streetscape Principles and Guidelines, and the Urban Open Space Plan. As also required, the proposed development's standards requested through the PAD overlay district take into consideration the location and context of the Site. Finally, as required, the proposed project appropriately mitigates transitional impacts on the immediate surroundings.

DEVELOPMENT PLAN REVIEW (DPR)

A Development Plan approval shall be based on the consideration of the following criteria:

Placement, form and articulation of buildings and structure provide variety in the streetscape.

The residential towers (the most predominate masses) are east-west facing but do not fill the entire site from east to west - allowing the building masses to offset each other and open views upwards from the open area at the corner of College Avenue and 7th Street. The Newman Office building is a smaller structure providing the transition between the historic Old Saint Mary's Church and the residential towers to the north. The tallest tower is mid-block and with the help of the horizontal base element transitions to the contemporary CAC building north of 7th Street. While the "lines" of the building complement those of the old church and CAC building, they also provide variety in between with a change in floor to floor heights, and vertical accents at the residential lobbies. See Development Concept section above.

Building design and orientation, together with landscape, combine to mitigate heat gain/retention while providing shade for energy conservation and human comfort.

The residential towers are east-west facing to address optimal solar orientation. The buildings are cantilevered over the ground level dining and retail opportunities to provide structured shade at these locations. Much of the mature vegetation along College Avenue will be saved in place, and enhanced with a similar street tree program along 7th Street for pedestrian comfort. On the Sky Park level (4) landscape is again added to reduce the heat gain from the top of the parking deck and to provide a comfortable outdoor setting. See Development Concept, Sky Park and Landscape sections above.

Materials are of superior quality, providing detail appropriate with their location and function while complementing the surroundings.

See Materials section above.

Building structures and landscape elements are appropriately scaled relative to the site and surroundings.

See Exhibit H – Proposed College Avenue Composite Elevation above. It shows the context of the proposed project with the surrounding approved towers and existing buildings.

Large building masses are sufficiently articulate so as to relieve monotony and create a sense of movement, resulting in a well-defined base and top, featuring enhanced pedestrian experience at and near street level.

See Development Concept section above.

Building faces provide architectural detail and interest overall within visibility at street level (in particular, special treatment of windows, entries and walkways with particular attention to proportionality, scale, materials rhythm, etc.) while responding to varying climatic and contextual conditions.

See Development Concept section above.

Plans take into account pleasant and convenient access to multi-modal transportation options and support the potential for transit patronage.

See Site Context and Surrounding Conditions section above.

Vehicular circulation is designed to minimize conflicts with pedestrian access and circulation, and with surrounding residential uses.

See Vehicle and Bicycle Parking and Resident and Commercial Loading sections above.

Plans appropriately integrate Crime Prevention Through Environmental Design principles, such as territoriality, natural surveillance, access control, activity support, and maintenance.

All plans have been reviewed by the Tempe Police department for appropriateness in this category. Changes to the plans have been made to incorporate their recommendations.

Landscape accents and provides delineation from parking, buildings, driveways and pathways.

See Landscape section above.

Signs have design, scale, proportion, location and color compatible with the design, colors, orientation and materials of the building or site on which they are located.

Signs are not included in this submission, but will be complementary or appropriate accents to the design, colors, orientation and materials of the building or site on which they are located.

Lighting is compatible with the proposed building and adjoining buildings and uses, and does not create negative effects.

See Lighting section above.

This Development Plan submittal for the Newman Center/The Maxwell on College meets the criteria for such structures as outlined above.

PLAT AND CROSS PARCEL BUILDING CODE EASEMENTS

We will be requesting to re-plot one of the owners parcels as it currently consists of four (4) individual lots. We are also aware that because we are building one structure across 4 different owners that an agreement to hold them in common for building code will be required by the City.

CONCLUSION

The Newman Center's Maxwell student housing project meets the approval requirements for a PAD and DPR. The innovative project will be the first of its kind in the state and a social and educational icon adding to the legacy and reputation of downtown Tempe. It will re-inforce Tempe's role as the leader in higher education in the state, and provide a new faith-based component to ASU's plan for a New American University. This will be a unique tool in Tempe's economic and cultural development, a center to attract, train, educate, support, house and retain as alumni - business and community leaders. We respectfully request the approval of the PAD and DPR for the Newman Center/The Maxwell on College student housing project.

PLANNED AREA DEVELOPMENT OVERLAY FOR ALL SAINTS NEWMAN CENTER

A PORTION OF THE SE QUARTER, SECTION 15, TOWNSHIP 1 NORTH, RANGE 4 EAST, OF THE GILA AND SALT RIVER BASE AND MERIDIAN, MARICOPA COUNTY, ARIZONA

ACKNOWLEDGMENT:

STATE OF ARIZONA
 COUNTY OF MARICOPA

ON THIS 17TH DAY OF DECEMBER 2007 BEFORE ME THE UNDERSIGNED PERSONALLY APPEARED FR. FRED LUCCL, WHO ACKNOWLEDGED HIMSELF TO BE THE PASTOR OF ALL SAINTS NEWMAN CENTER AND THAT HE BEING AUTHORIZED TO DO SO, EXECUTED THE FOREGOING INSTRUMENT IN THE CAPACITY THEREIN STATED FOR THE PURPOSES THEREIN CONTAINED.

IN WITNESS WHEREOF, I HEREUNTO SET MY HAND AND OFFICIAL SEAL.

NOTARY PUBLIC

MY COMMISSION EXPIRES:

LEGAL DESCRIPTION

Parcel 1

The east 30 feet of Lot 17 and all Lots 18, 19, and 20, Block 13 Town of Tempe, also the north 16 feet of 8th street lying adjacent on the south side of above described lots according to the plat in book 2 of maps, page 26, Maricopa County, Arizona.

Parcel 2

Parcel No. 1: The west 10 feet of Lot seventeen (17), and Lot Sixteen (16), except the west 3 feet, block thirteen (13), Tempe, in the City of Tempe, according to the plat in book 2 of maps, page 26. Parcel No. 2: that portion of 8th street now vacated and abandoned by the City of Tempe adjoining portions of Lots 16 and 17, Block 13, City of Tempe West, according to the plat of record in the office of the county recorder in book 2 of maps, page 26 described as follows:

Beginning at a point on the south line of said Lot 16 which is 3 feet east of the southwest corner thereof and running thence east, along the south line of said Lots 16 and Seventeen, a distance of 47 feet to a point which is 10 feet east of the southwest corner of said Lot 17; thence south, on a line parallel to the west line of said Lot 17, a distance of 16 feet to the north line of 8th street as it now exists; thence westerly, along the north line of 8th street as it now exists, a distance of 47 feet to a point in a line which is 3 feet east of and parallel to the west line of said Lot 16; thence north along said parallel line, a distance of 16 feet to the point of beginning.

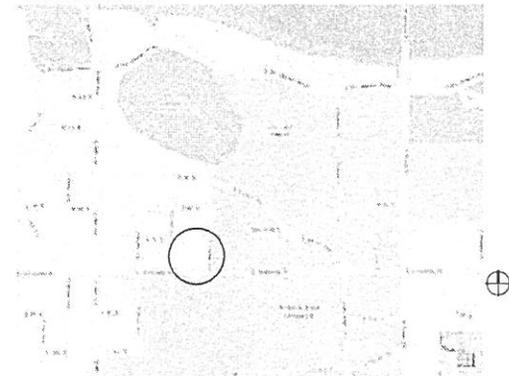
OWNER:

ALL SAINTS NEWMAN CENTER
 230 EAST UNIVERSITY DRIVE
 TEMPE, AZ 85281
 480.967.7823

DEVELOPER:

DOMUS COMMUNITIES
 30 WEST PERSHING ROAD
 SUITE 201
 KANSAS CITY, MO 64180
 866.543.6687

LOCATION MAP



CONDITIONS OF APPROVAL:

GENERAL NOTES:

PROJECT DATA:

SITE AREA: 37,455 SF
 BUILDING AREA: 258,588 SF
 PROPOSED USES: MULTIPLE FAMILY DWELLINGS, RELIGIOUS ASSEMBLY, CLASSROOMS
 # OF UNITS: 180
 # OF STORIES: 22
 BUILDING HEIGHT: 243'-2"

APPROVAL:

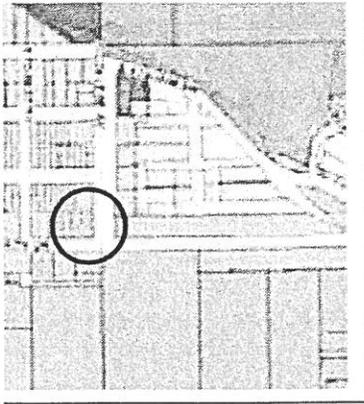
BY: _____ DATE: _____
 DEVELOPMENT SERVICES MANAGER

ATTACHMENT 65



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lap



and 20, Block 13 Town of
g adjacent on the south side of
ook 2 of maps, page 26, Mari-

in (17), and Lot Sixteen (16),
pe, in the City of Tempe,
26. Parcel No. 2: that portion
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er in book 2 of maps, page 26

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o a point which is 10 feet east
south, on a line parallel to the
o the north line of 8th street as it
ie of 8th street as it now exists,
s 3 feet east of and parallel to
y said parallel line, a distance of

Site Data

A.	SUBMITTED BY: NAME ADDRESS PHONE	DOMUS COMMUNITIES 30 WEST PERSHING ROAD SUITE 201 KANSAS CITY, MO 64108 866.543.6687																																																																																
B.	PROJECT NAME:	ASU NEWMAN CENTER																																																																																
C.	SITE ADDRESS:	230 EAST UNIVERSITY DRIVE TEMPE, ARIZONA																																																																																
D.	SCALE, NORTH ARROW	SEE SITE PLAN SHEET																																																																																
E.	LOCATION MAP	SEE GRAPHIC THIS SHEET																																																																																
F.	PROPERTY LINES/LEASE LINES/PHASE LINES	SEE SITE PLAN SHEET																																																																																
G.	LEGAL DESCRIPTION	SEE DESCRIPTION THIS SHEET																																																																																
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I.	ZONING: EXISTING PROPOSED	CITY CENTER (TOD) CITY CENTER - PAD (TOD)																																																																																
J.	PARCEL SIZE (NET & GROSS)	37,455 SF (.86 ACRES)																																																																																
K.	BUILDING AREA PERCENTAGE OF LOT COVERAGE HEIGHT OF BUILDING NUMBERS OF STORIES	258,588 SF (INCLUDING BASEMENT) 63% (INCLUDES EXISTING CHURCH TO REMAIN) 243'-2" 22																																																																																
L.	TYPE OF CONSTRUCTION PER INTERNATIONAL BUILDING CODE	TYPE 1-A* (Table 503) * Type 1-A construction may be reduced to type 1-B The height of the reduced construction type shall be the same as for the original construction type (403.3.1-3)																																																																																
M.	AUTOMATIC EXTINGUISHING SYSTEM	The Building will be equipped with an automatic extinguishing system per City of Tempe IBC.																																																																																
N.	PROPOSED USES	MULTIPLE FAMILY DWELLINGS, RELIGIOUS ASSEMBLY, UNIVERSITY CLASSROOMS																																																																																
O.	NUMBER AND TYPE OF RESIDENTIAL UNITS	1 BD 12 2 BD 84 3 BD 84 Total 180																																																																																
P.	DENSITY	209 DWELLING UNITS/ACRE																																																																																
Q.	PARKING FOR MOTOR VEHICLES AND BICYCLES REQUIRED & PROVIDED	<table border="1"> <thead> <tr> <th>PARKING</th> <th>TOD</th> <th>ULI</th> <th>ITE</th> <th>PROVIDED</th> </tr> </thead> <tbody> <tr> <td>1 BR APARTMENT (.75 /UNIT)</td> <td>9</td> <td>9</td> <td>2</td> <td>3</td> </tr> <tr> <td>2 BR APARTMENT (1.5 /UNIT)</td> <td>126</td> <td>63</td> <td>46</td> <td>42</td> </tr> <tr> <td>3 BR APARTMENT (2.25 /UNIT)</td> <td>189</td> <td>63</td> <td>46</td> <td>63</td> </tr> <tr> <td>GUEST (.2 /UNIT)</td> <td>36</td> <td></td> <td></td> <td>12</td> </tr> <tr> <td>CHURCH (1 PER 100 SF)</td> <td>67</td> <td></td> <td></td> <td>0</td> </tr> <tr> <td>OFFICE (1 PER 300 SF)</td> <td>13</td> <td></td> <td></td> <td>18</td> </tr> <tr> <td>CLASSROOM (1 PER 200 SF)</td> <td>21</td> <td></td> <td></td> <td>0</td> </tr> <tr> <td>LESS SHARED PARKING*</td> <td>(35)</td> <td></td> <td></td> <td>9</td> </tr> <tr> <td>TOTAL</td> <td>426</td> <td>135</td> <td>94</td> <td>147</td> </tr> </tbody> </table> <p>*SHARED PARKING INCLUDES OFFICE AND CLASSROOM SPACES SINCE THEY WILL NOT BE USED SIMULTANEOUSLY WITH THE CHURCH</p> <p>SEE ATTACHED LETTER REGARDING PARKING ACCOMMODATIONS</p> <table border="1"> <thead> <tr> <th>BIKE COMMUTE AREA</th> <th>REQUIRED</th> <th>PROVIDED</th> </tr> </thead> <tbody> <tr> <td>1 BR APARTMENT (.75 /UNIT)</td> <td>9</td> <td>18</td> </tr> <tr> <td>2 BR APARTMENT (.75 /UNIT)</td> <td>63</td> <td>126</td> </tr> <tr> <td>3 BR APARTMENT (1 /UNIT)</td> <td>84</td> <td>168</td> </tr> <tr> <td>GUEST (.2 SPACES/UNIT)</td> <td>36</td> <td>36</td> </tr> <tr> <td>CHURCH (1 SPACE PER 1500 SF)</td> <td>9</td> <td>18</td> </tr> <tr> <td>OFFICE (1 SPACE PER 8000 SF; 2 MIN.)</td> <td>2</td> <td>4</td> </tr> <tr> <td>CLASSROOM (1 SPACE PER 1500 SF)</td> <td>3</td> <td>6</td> </tr> <tr> <td>LESS SHARED PARKING*</td> <td>0</td> <td>0</td> </tr> <tr> <td>TOTAL</td> <td>206</td> <td>376</td> </tr> </tbody> </table>	PARKING	TOD	ULI	ITE	PROVIDED	1 BR APARTMENT (.75 /UNIT)	9	9	2	3	2 BR APARTMENT (1.5 /UNIT)	126	63	46	42	3 BR APARTMENT (2.25 /UNIT)	189	63	46	63	GUEST (.2 /UNIT)	36			12	CHURCH (1 PER 100 SF)	67			0	OFFICE (1 PER 300 SF)	13			18	CLASSROOM (1 PER 200 SF)	21			0	LESS SHARED PARKING*	(35)			9	TOTAL	426	135	94	147	BIKE COMMUTE AREA	REQUIRED	PROVIDED	1 BR APARTMENT (.75 /UNIT)	9	18	2 BR APARTMENT (.75 /UNIT)	63	126	3 BR APARTMENT (1 /UNIT)	84	168	GUEST (.2 SPACES/UNIT)	36	36	CHURCH (1 SPACE PER 1500 SF)	9	18	OFFICE (1 SPACE PER 8000 SF; 2 MIN.)	2	4	CLASSROOM (1 SPACE PER 1500 SF)	3	6	LESS SHARED PARKING*	0	0	TOTAL	206	376
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R.	LANDSCAPE AREA ON-SITE	5,355 sf; 14%																																																																																
S.	PAD OVERLAY	SEE SITE PLAN SHEET																																																																																
T.	CLOSEST FIRE HYDRANT	SEE SITE PLAN SHEET																																																																																
U.	ALL EXISTING & PROPOSED REFUSE ENCLOSURES	SEE SITE PLAN SHEET																																																																																
V.	ALL STREETS, MEDIANS & DRIVEWAYS (BOTH SIDES OF STREET) WITHIN 125' OF PROPERTY	SEE SITE PLAN SHEET																																																																																

IN-PROGRES



PROJECT DATA

ALL SAINTS NEWMAN CENTER

Domus Communities

10.09.2007

02.01.2008 revised 12.18.2007 revised

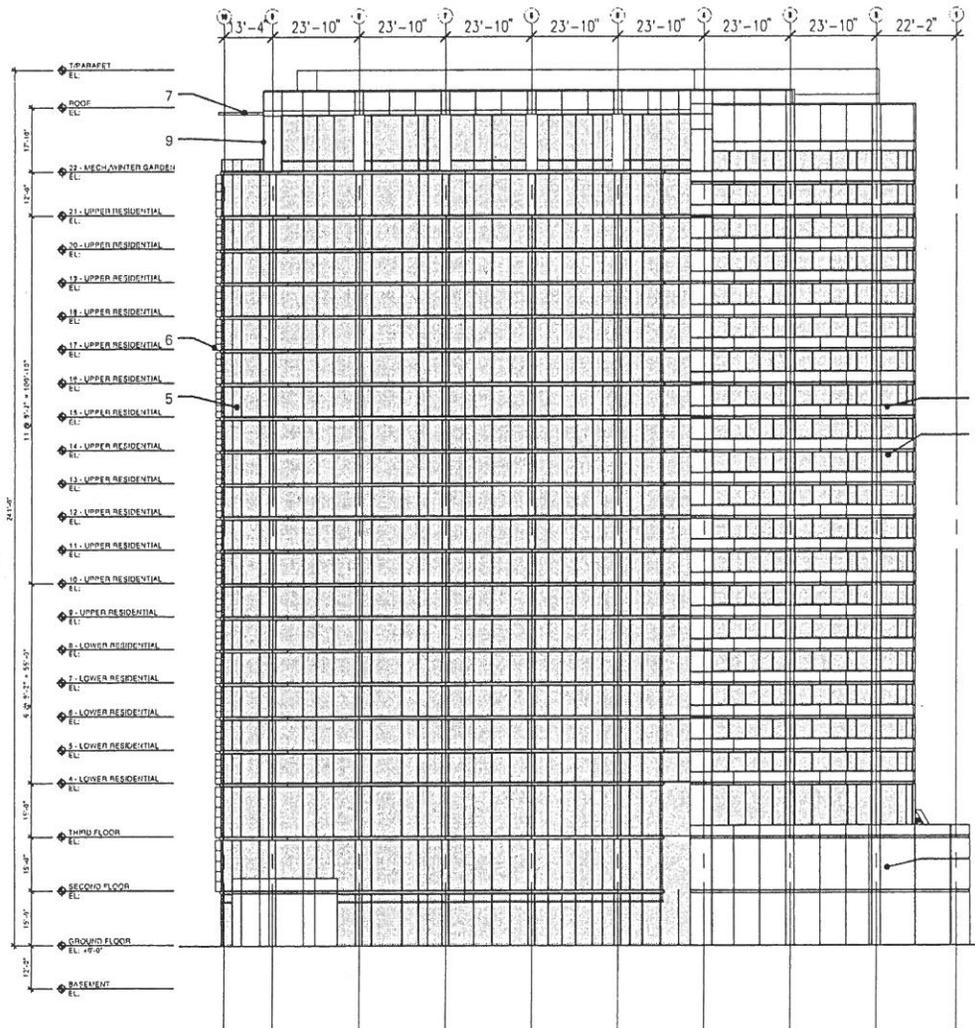
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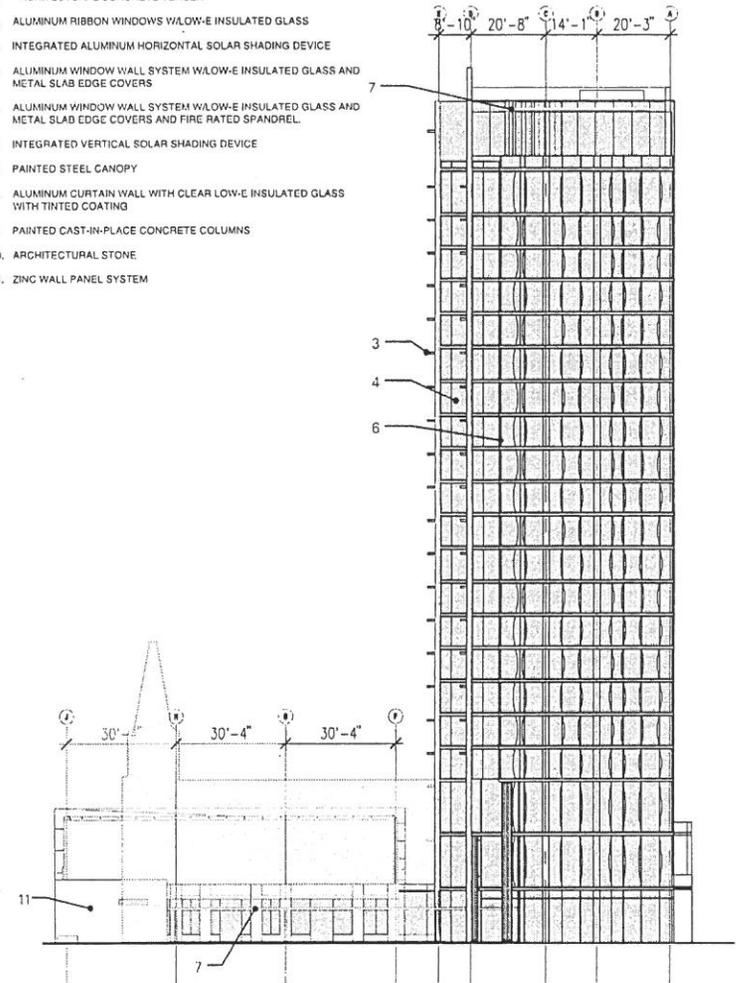
KEY

1. ARCHITECTURAL CONCRETE VENEER
2. ALUMINUM RIBBON WINDOWS W/LOW-E INSULATED GLASS
3. INTEGRATED ALUMINUM HORIZONTAL SOLAR SHADING DEVICE
4. ALUMINUM WINDOW WALL SYSTEM W/LOW-E INSULATED GLASS AND METAL SLAB EDGE COVERS
5. ALUMINUM WINDOW WALL SYSTEM W/LOW-E INSULATED GLASS AND METAL SLAB EDGE COVERS AND FIRE RATED SPANDREL
6. INTEGRATED VERTICAL SOLAR SHADING DEVICE
7. PAINTED STEEL CANOPY
8. ALUMINUM CURTAIN WALL WITH CLEAR LOW-E INSULATED GLASS WITH TINTED COATING
9. PAINTED CAST-IN-PLACE CONCRETE COLUMNS
10. ARCHITECTURAL STONE
11. ZINC WALL PANEL SYSTEM



KEY

1. ARCHITECTURAL CONCRETE VENEER
2. ALUMINUM RIBBON WINDOWS W/LOW-E INSULATED GLASS
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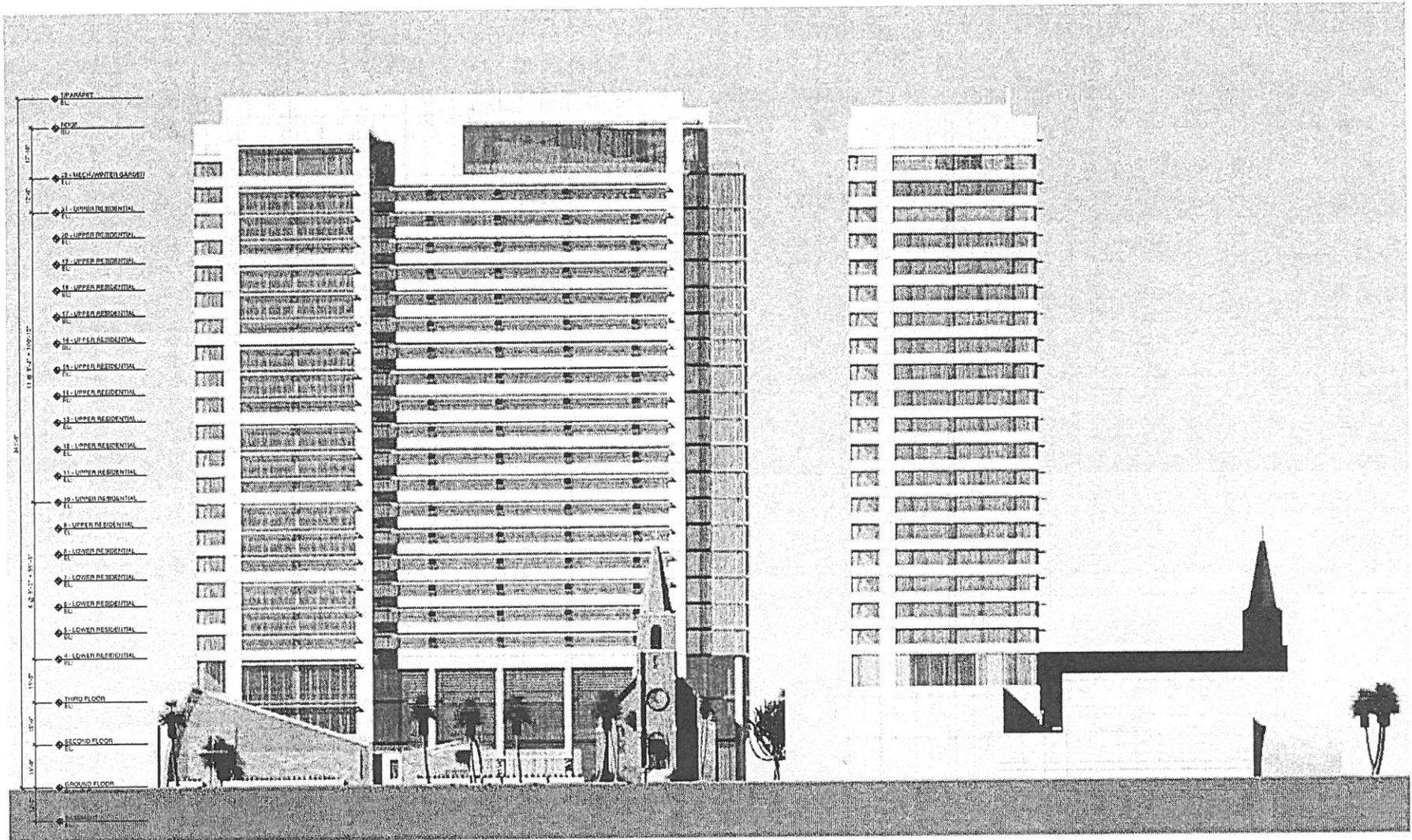
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NORTH & EAST ELEVATIONS
 ALL SAINTS NEWMAN CENTER
 Domus Communities

10.09.2007
 12.18.2007 revised
 2007023

IN-PROGRESS
 0 16' 32'
 18



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SOUTH & WEST ELEVATIONS
 ALL SAINTS NEWMAN CENTER
 Domus Communities

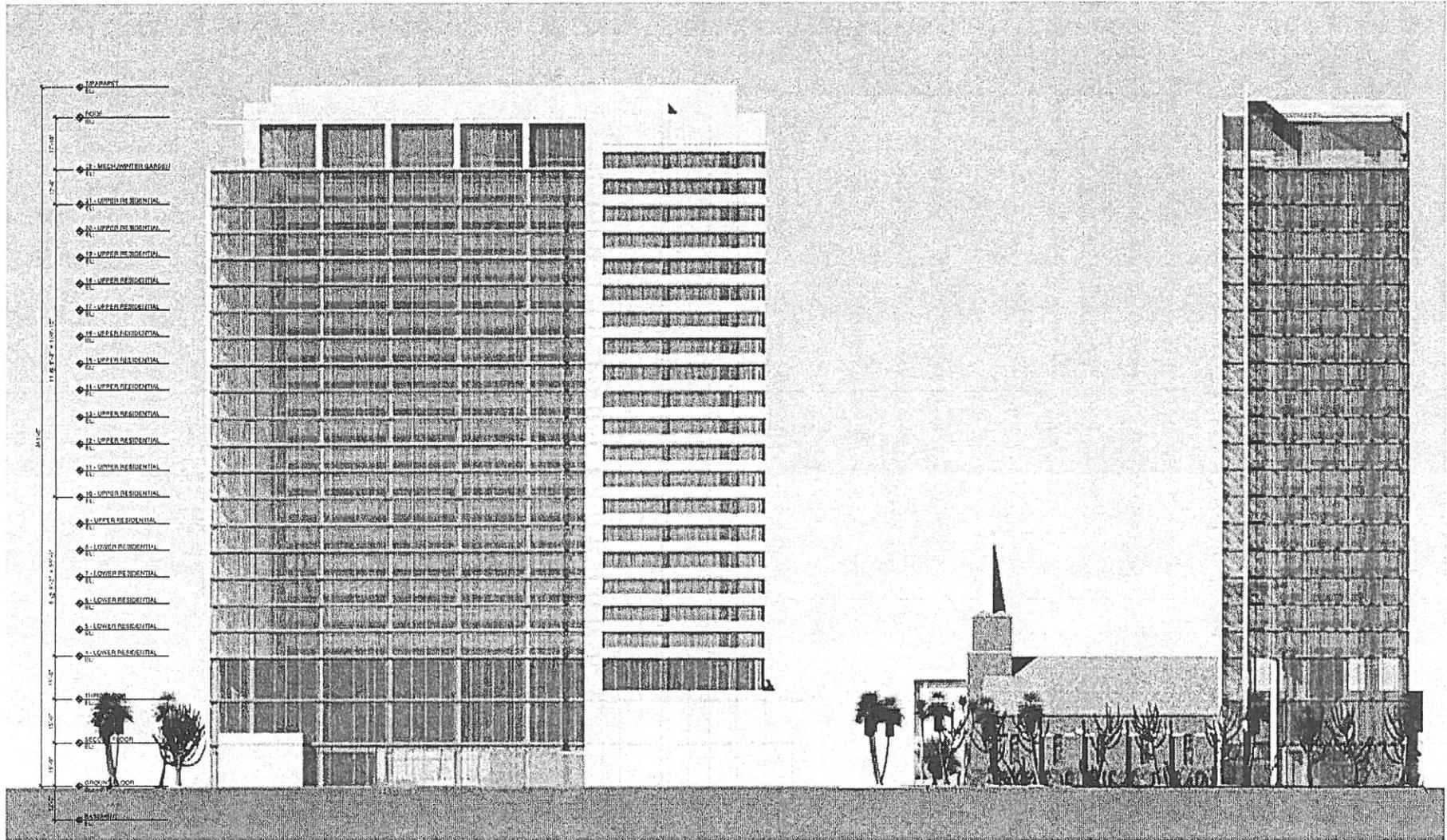
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 12.18.2007 revised
 2007023

IN-PROGRESS

0 16' 32'



19



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NORTH & EAST ELEVATIONS
 ALL SAINTS NEWMAN CENTER
 Domus Communities

IN-PROGRESS
 10.09.2007
 12.18.2007 revised
 2007023
 0 16' 32'
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VIEW FROM UNIVERSITY DRIVE
ALL SAINTS NEWMAN CENTER
Domus Communities

10.09.2007
12.16.2007 revised
2007023

IN-PROGRESS



ACKNOWLEDGEMENT

ON THIS _____ DAY OF _____, 2016
BEFORE ME, THE UNDERSIGNED, PERSONALLY APPEARED _____, WHO ACKNOWLEDGED HIMSELF TO BE THE PERSON
WHOSE NAME IS SUBSCRIBED TO THE INSTRUMENT WITHIN,
AND WHO EXECUTED THE FOREGOING INSTRUMENT FOR
THE PURPOSES THEREIN CONTAINED.

IN WITNESS WHEREOF, I HEREUNTO SET MY HAND AND OFFICIAL SEAL

BY: _____ NOTARY PUBLIC MY COMMISSION EXPIRES _____
COLLEGE ENTERPRISES, INC.:

BY: _____ OWNER _____ DATE _____

ON THIS _____ DAY OF _____, 2016
BEFORE ME, THE UNDERSIGNED, PERSONALLY APPEARED _____, WHO ACKNOWLEDGED HIMSELF TO BE THE PERSON
WHOSE NAME IS SUBSCRIBED TO THE INSTRUMENT WITHIN,
AND WHO EXECUTED THE FOREGOING INSTRUMENT FOR
THE PURPOSES THEREIN CONTAINED.

IN WITNESS WHEREOF, I HEREUNTO SET MY HAND AND OFFICIAL SEAL

BY: _____ NOTARY PUBLIC MY COMMISSION EXPIRES _____
COLLEGE STREET, L.L.C., AN ARIZONA LIMITED LIABILITY COMPANY:

BY: _____ OWNER _____ DATE _____

ON THIS _____ DAY OF _____, 2016
BEFORE ME, THE UNDERSIGNED, PERSONALLY APPEARED _____, WHO ACKNOWLEDGED HIMSELF TO BE THE PERSON
WHOSE NAME IS SUBSCRIBED TO THE INSTRUMENT WITHIN,
AND WHO EXECUTED THE FOREGOING INSTRUMENT FOR
THE PURPOSES THEREIN CONTAINED.

IN WITNESS WHEREOF, I HEREUNTO SET MY HAND AND OFFICIAL SEAL

BY: _____ NOTARY PUBLIC MY COMMISSION EXPIRES _____
SABA BROTHERS RENTALS, LLC.:

BY: _____ OWNER _____ DATE _____

ON THIS _____ DAY OF _____, 2016
BEFORE ME, THE UNDERSIGNED, PERSONALLY APPEARED _____, WHO ACKNOWLEDGED HIMSELF TO BE THE PERSON
WHOSE NAME IS SUBSCRIBED TO THE INSTRUMENT WITHIN,
AND WHO EXECUTED THE FOREGOING INSTRUMENT FOR
THE PURPOSES THEREIN CONTAINED.

IN WITNESS WHEREOF, I HEREUNTO SET MY HAND AND OFFICIAL SEAL

BY: _____ NOTARY PUBLIC MY COMMISSION EXPIRES _____
ALL SAINTS ROMAN CATHOLIC NEWMAN CENTER TEMPE:

BY: _____ OWNER _____ DATE _____

LEGAL DESCRIPTION

LOTS 1, 2 AND 3 OF BLOCK 13, OF TEMPE, ACCORDING TO THE PLAT OF RECORD IN THE OFFICE OF THE MARICOPA COUNTY, RECORDER IN BOOK 2 OF MAPS, PAGE 26; EXCEPT THE NORTH 55.1 FEET THEREOF;

TOGETHER WITH
LOT 1, ALL SAINTS ROMAN CATHOLIC NEWMAN CENTER, ACCORDING TO THE PLAT THEREOF RECORDED IN BOOK 1089 OF MAPS, PAGE 33, RECORDS OF MARICOPA COUNTY, ARIZONA.

TOGETHER WITH
LOTS 4, 5, 6 AND 7, BLOCK 13, TEMPE, ACCORDING TO BOOK 2 OF MAPS, PAGE 26, RECORDS OF MARICOPA COUNTY, ARIZONA.

APPROVAL

APPROVED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF TEMPE
ON THIS _____ DAY OF _____, 2016.

**PLANNED AREA DEVELOPMENT OVERLAY FOR
NEWMAN CENTER / THE MAXWELL ON COLLEGE**

A PORTION OF THE SOUTHWEST QUARTER, OF SECTION 15, TOWNSHIP 1N, RANGE 4E, OF THE GILA AND SALT RIVER BASE AND MERIDIAN, MARICOPA COUNTY, ARIZONA

OWNER

COLLEGE STREET, L.L.C.
712 S. COLLEGE AVE
TEMPE, AZ 85281

COLLEGE ENTERPRISES, INC.
704 S. COLLEGE AVE
TEMPE, AZ 85281

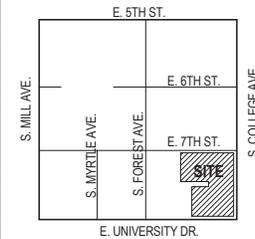
SABA BROTHERS RENTALS, LLC.
704 S. COLLEGE AVE
TEMPE, AZ 85281

ALL SAINTS ROMAN CATHOLIC NEWMAN CENTER TEMPE
230 E. UNIVERSITY DR
TEMPE, AZ 85281

PROJECT DATA

ZONING: CC PAD, TOD, HP TABLE 4-203B, 5-611A	PROPOSED SITE A***	PROPOSED SITE B***	PAD PROVIDED (SITE A & B)
GENERAL PLAN LAND USE (2040 PLAN)	MIXED USE	MIXED USE	MIXED USE
GENERAL PLAN DENSITY (2040 PLAN)	HIGH DENSITY URBAN CORE (MORE THAN 65 DU/AC)	HIGH DENSITY URBAN CORE (MORE THAN 65 DU/AC)	HIGH DENSITY URBAN CORE (MORE THAN 65 DU/AC)
SITE AREA	45,637 SF / 1.05 AC	36,488 SF / 0.84 AC	82,125 SF / 1.89 AC
DWELLING QUANTITY:	294 DWELLING UNITS	1 DWELLING UNIT	295 DWELLING UNITS
DENSITY:	281 DU / AC	2 DU / AC	156 DU / AC
BUILDING HEIGHT:	245'-0"	75'-0" (EXISTING, HISTORIC OLD ST MARY'S EXCEPTED)	245'-0"
BUILDING LOT COVERAGE:	100%	83%	92%
SITE LANDSCAPE COVERAGE - GROUND LEVEL**	0%	17.8% (6,500 SF)	7.9% (6,500 SF)
SITE LANDSCAPE COVERAGE - DECK LEVEL**	41.2% (18,800 SF)		22.9% (18,800 SF)
FRONT SETBACK	0'-0" (EAST) 15' BUILDING ENCROACHMENT INTO ROW	0'-0" (SOUTH)	0'-0"
SIDE SETBACK	0'-0" (SOUTH)	0'-0" (WEST)	0'-0"
STREET SIDE SETBACK	0'-0" (NORTH) 12' BUILDING ENCROACHMENT INTO ROW	0'-0" (EAST) 15' BUILDING ENCROACHMENT INTO ROW	0'-0"
REAR SETBACK	0'-0" (WEST)	0'-0" (NORTH)	0'-0"
VEHICLE PARKING QUANTITY			
1 BEDROOM (23 UNITS)	11.50 (0.5/BED)	0	11.50 (0.5/BED)
2 BEDROOM (156 UNITS)	68.64 (0.22/BED)	0	68.64 (0.22/BED)
3 BEDROOM (1 UNIT)	0	0 (0.0/BED - LIVE/WORK RECTORY)	0 (0.0/BED - LIVE/WORK RECTORY)
4 BEDROOM (115 UNITS)	0 (0.0/BED)	0	0 (0.0/BED)
GUEST	29.40 (0.1/UNIT)	0 (0.1/UNIT)	29.40 (0.1/UNIT)
COMMERCIAL (OFFICE, RESTAURANT, RETAIL)	74.70 (-5000 SF, 1/500)	23.71 (1/500)	98.41 (-5000 SF, 1/500)
CLASSROOM	0	9.28 (1/350)	9.28 (1/350)
CHURCH (EXISTING)	0	0 (0/300 - PROVIDED OFF SITE ON WEEKENDS)	0 (0/300 - PROVIDED OFF SITE ON WEEKENDS)
OFFICE (EXISTING)	0	0 (0/500)	0 (0/500)
VEHICLE PARKING PROVIDED:	217 (10 ON STREET)	0	217 (10 ON STREET)
BICYCLE PARKING QUANTITY			
1 BEDROOM (23 UNITS)	23 (1/BED)	0	23 (1/BED)
2 BEDROOM (156 UNITS)	312 (1/BED)	0	312 (1/BED)
3 BEDROOM (1 UNIT)	0	3 (1/BED)	3 (1/BED)
4 BEDROOM (115 UNITS)	460 (1/BED)	0	460 (1/BED)
GUEST	0 (0.0/UNIT)	0 (0.0/UNIT)	0 (0.0/UNIT)
OFFICE	4 (1/8000, 4 MIN)	0	4 (1/8000, 4 MIN)
RESTAURANT	29.04 (1/500)	5.41 (1/500)	34.45 (1/500)
RETAIL	4 (1/7500, 4 MIN)	0	4 (1/7500, 4 MIN)
CLASSROOM	0	2.17 (1/1500)	2.17 (1/1500)
CHURCH (EXISTING)	0	12.20 (1/1500)	12.20 (1/1500)
OFFICE (EXISTING)	0	4 (1/8000, 4 MIN)	4 (1/8000, 4 MIN)
BICYCLE PARKING PROVIDED:	832 (37 ON STREET, SEMI PUBLIC)	27 (ON STREET)	859 (64 ON STREET)
USES			
RESIDENTIAL	472,953 SF	4,903 SF	477,856 SF
OFFICE*	14,960 SF	9,153 SF	24,113 SF
RETAIL*	12,870 SF	-	12,870 SF
RESTAURANT*	14,520 SF	2,703 SF	17,223 SF
CLASSROOM	-	3,249 SF	3,249 SF
POOL & OUTDOOR AMENITY	18,800 SF	-	18,800 SF
STRUCTURED PARKING	115,000 SF	-	115,000 SF
CHURCH (EXISTING)****	-	18,306 SF	18,306 SF
OFFICE (EXISTING)****	-	3,345 SF	3,345 SF
TOTAL	649,103 SF	41,659 SF	690,762 SF

SITE VICINITY MAP



REC15144

PL150419

DS150915

CONDITIONS OF APPROVAL:

Date	Description
02.22.16	Amended Site Plan Submittal
03.14.16	Amended Site Plan Submittal

PAD NOTES:

- * THESE COMMERCIAL USES ARE NOT LIMITED BY THE PAD AND MAY BE CONFIGURED DIFFERENTLY.
- ** THESE AREAS INCLUDE HARDSCAPE PEDESTRIAN AREAS AND POOL.
- *** THESE SITES ARE NOT LIMITED BY THE PAD AND MAY BE CONFIGURED DIFFERENTLY.
- **** EXISTING AREAS WERE PROVIDED BY MARICOPA COUNTY ASSESSOR'S OFFICE.

NOTES:

Seal/Signature



Project Name

Newman Center / The Maxwell on College

Project Number

57,6119.000

Description

PAD COVER

Scale

12" = 1'-0"

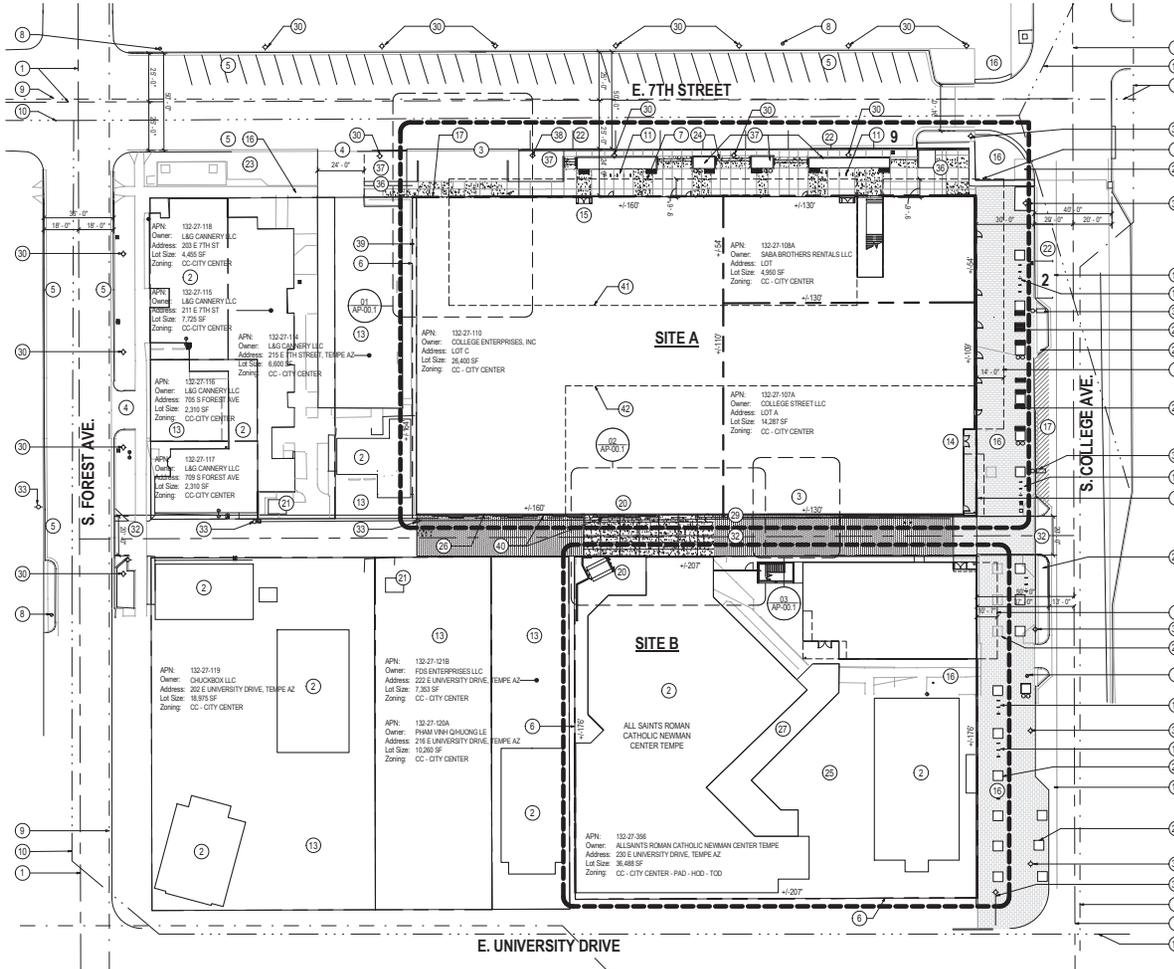
PAD-01

DS150915

PL150419

REC15144

PLANNED AREA DEVELOPMENT OVERLAY FOR NEWMAN CENTER / THE MAXWELL ON COLLEGE



01 SITE PLAN - LEVEL 01
SCALE: 1" = 30'-0"

DS150915

PL150419

REC15144

- 1 STREET CENTERLINE
- 2 EXISTING BUILDING TO REMAIN
- 3 VEHICULAR DRIVEWAY ENTRY
- 4 EXISTING DRIVEWAY
- 5 EXISTING ON-STREET PARKING
- 6 PROPERTY LINE
- 7 BUILDING OVERHANG
- 8 EXISTING FIRE HYDRANT
- 9 EXISTING WATER LINE
- 10 EXISTING SEWER LINE
- 11 BIKE PARKING
- 12 EXISTING BICYCLE PARKING
- 13 EXISTING PARKING LOT
- 14 SOUTH TOWER ENTRANCE
- 15 NORTH TOWER ENTRANCE
- 16 EXISTING SIDEWALK
- 17 COMMERCIAL LOADING ZONE
- 18 VEHICULAR PARKING
- 19 EXISTING BIKE LANE
- 20 REFUSE COLLECTION AREA
- 21 EXISTING REFUSE ENCLOSURE
- 22 ON-STREET PARKING
- 23 EXISTING PLANTER
- 24 SEATING AREA
- 25 EXISTING COURTYARD
- 26 ELECTRICAL EQUIPMENT TO BE ACCESSED FROM ALLEY
- 27 EXISTING ARCADE
- 28 ACCESSIBLE PATH
- 29 GAS METER
- 30 EXISTING STREET LIGHT
- 31 EXISTING TRAFFIC LIGHT
- 32 EXISTING ALLEY
- 33 EXISTING UTILITY POLE
- 34 NEW UTILITY POLE
- 35 NEW STREET LIGHT
- 36 NEW SIDEWALK
- 37 NEW LANDSCAPE PLANTER
- 38 RELOCATED STREET LIGHT
- 39 DECOMPOSED GRANITE
- 40 SLIDING ACCESS GATES
- 41 NORTH TOWER ABOVE
- 42 SOUTH TOWER ABOVE

- SITE PLAN LEGEND**
- PROPERTY LINE
 - - - STREET CENTERLINE
 - - - PROJECT LIMITS
 - - - EXISTING WATER LINE
 - - - EXISTING SEWER LINE
 - ⊕ EXISTING FIRE HYDRANT
 - ⊙ EXISTING STREET LIGHT
 - STREET LIGHT

REC15144

PL150419

DS150915

The Maxwell Tempe, LLC
712 S College Avenue
Tempe, AZ 85281

Gensler

301 East Washington St
Suite 750
Phoenix, AZ 85004
United States
Tel: 602.523.4900
Fax: 602.523.4949

Date	Description
02.22.16	Amended Site Plan Submittal
03.14.16	Amended Site Plan Submittal

Seal/Signature



Project Name
Newman Center / The Maxwell on College

Project Number
57.6119.000

Description
PAD SITE PLAN

Scale
As indicated

PAD-02

ZONING: CC PAD, TOD, HP TABLE 4-203B, 5-611A	TOD CORRIDOR (SITE A & B)	EXISTING PAD SITE B	PROPOSED SITE A ***	PROPOSED SITE B***	PAD PROVIDED (SITE A & B)
GENERAL PLAN LAND USE	MIXED USE	MIXED USE	MIXED USE	MIXED USE	MIXED USE
GENERAL PLAN DENSITY	HIGH DENSITY URBAN CORE (MORE THAN 65 DU/AC)	HIGH DENSITY URBAN CORE (MORE THAN 65 DU/AC)	HIGH DENSITY URBAN CORE (MORE THAN 65 DU/AC)	HIGH DENSITY URBAN CORE (MORE THAN 65 DU/AC)	HIGH DENSITY URBAN CORE (MORE THAN 65 DU/AC)
SITE AREA	82,125 SF / 1.89 AC	37,462 SF / 0.86 AC	45,637 SF / 1.05 AC	36,488 SF / 0.84 AC	82,125 SF / 1.89 AC
DWELLING QUANTITY	MORE THAN 123 DWELLING UNITS	180 DWELLING UNITS 432 BEDROOMS	294 DWELLING UNITS 795 BEDROOMS	1 DWELLING UNIT 3 BEDROOMS	295 DWELLING UNITS 798 BEDROOMS
DENSITY	65+ DU / AC	214.3 DU / AC	281 DU / AC	2 DU / AC	156 DU / AC
BUILDING HEIGHT	50' MAX	244'-0" 270'-0" AS APPROVED PER 07027 CONDITION 5	245'-0"	75'-0" (EXISTING, HISTORIC OLD ST MARY'S EXCEPTED)	245'-0"
BUILDING LOT COVERAGE	NS	63%	100%	83%	92%
SITE LANDSCAPE COVERAGE - GROUND LEVEL**	NS	14%	0%	17.8% (6,500 SF)	7.9% (6,500 SF)
SITE LANDSCAPE COVERAGE - DECK LEVEL**	NOT REQUIRED		41.2% (18,800 SF)		22.9% (18,800 SF)
FRONT SETBACK	0'-0"	7' (EAST)	0'-0" (EAST) 15' BUILDING ENCROACHMENT INTO ROW	0'-0" (SOUTH)	0'-0"
SIDE SETBACK	0'-0"	0' (NORTH)	0'-0" (SOUTH)	0'-0" (WEST)	0'-0"
STREET SIDE SETBACK	0'-0"	0' (SOUTH)	0'-0" (NORTH) 12' BUILDING ENCROACHMENT INTO ROW	0'-0" (EAST) 15' BUILDING ENCROACHMENT INTO ROW	0'-0"
REAR SETBACK	0'-0"	0' (WEST)	0'-0" (WEST)	0'-0" (NORTH)	0'-0"
VEHICLE PARKING QUANTITY					
RESIDENTIAL USE:					
1 BEDROOM (23 UNITS)	11.50 (0.5/BED)		11.50 (0.5/BED)	0	11.50 (0.5/BED)
2 BEDROOM (156 UNITS)	156.00 (0.5/BED)		68.64 (0.22/BED)	0	68.64 (0.22/BED)
3 BEDROOM (1 UNIT)	0.90 (0.3/BED)		0	0 (0.0/BED - LIVE/WORK RECTORY)	0 (0.0/BED - LIVE/WORK RECTORY)
4 BEDROOM (115 UNITS)	138.00 (0.3/BED)		0 (0.0/BED)	0	0 (0.0/BED)
TOTAL RESIDENTS	306.40		73.90	0	73.90
GUEST	29.50 (0.1/UNIT)		29.40 (0.1/UNIT)	0 (0.1/UNIT)	29.40 (0.1/UNIT)
TOTAL RESIDENTIAL	335.90		109.54	0	110.54
COMMERCIAL (OFFICE, RESTAURANT, RETAIL)	98.41 (-5000, 1/500)		74.70 (-5000 SF, 1/500)	23.71 (1/500)	98.41 (-5000 SF, 1/500)
CLASSROOM	9.28 (1/350)		0	9.28 (1/350)	9.28 (1/350)
CHURCH (EXISTING)	0 (0/300 - PROVIDED OFF SITE ON WEEKENDS)		0	0 (0/300 - PROVIDED OFF SITE ON WEEKENDS)	0 (0/300 - PROVIDED OFF SITE ON WEEKENDS)
OFFICE (EXISTING)	0 (0/500 - CONVERTED TO STORAGE)		0	0 (0/500)	0 (0/500)
TOTAL VEHICLE PARKING PROVIDED	516	530	217 (10 ON STREET)	0	217 (10 ON STREET)
BICYCLE PARKING QUANTITY		147, 50 DEDICATED ON-SITE OR CONTIGUOUS LOT & 3 SPACES FOR SHARED CAR PROGRAM			
RESIDENTIAL USE:					
1 BEDROOM (23 UNITS)	17.25 (0.75/UNIT)		23 (1/BED)	0	23 (1/BED)
2 BEDROOM (156 UNITS)	117.00 (0.75/UNIT)		312 (1/BED)	0	312 (1/BED)
3 BEDROOM (1 UNIT)	1.00 (1/UNIT)		0	3 (1/BED)	3 (1/BED)
4 BEDROOM (115 UNITS)	115.00 (1/UNIT)		460 (1/BED)	0	460 (1/BED)
TOTAL RESIDENTS	250.25		795	3	798
GUEST	59.00 (0.2/UNIT)		0 (0.0/UNIT)	0 (0.0/UNIT)	0 (0.0/UNIT)
TOTAL RESIDENTIAL	309		795	3	798
OFFICE	4 (1/8000, 4 MIN)		4 (1/8000, 4 MIN)	0	4 (1/8000, 4 MIN)
RESTAURANT	34.45 (1/1500)		29.04 (1/500)	5.41 (1/500)	34.45 (1/500)
RETAIL	4 (1/7500, 4 MIN)		4 (1/7500, 4 MIN)	0	4 (1/7500, 4 MIN)
CLASSROOM	2.17 (1/1500)		0	2.17 (1/1500)	2.17 (1/1500)
CHURCH (EXISTING)	12.20 (1/1500)		0	12.20 (1/1500)	12.20 (1/1500)
OFFICE (EXISTING)	4 (1/8000, 4 MIN)		0	4 (1/8000, 4 MIN)	4 (1/8000, 4 MIN)
TOTAL BICYCLE PARKING PROVIDED	363	366	832 (37 ON STREET, SEMI PUBLIC)	27 (ON STREET)	859 (64 ON STREET)
USES					
RESIDENTIAL	477,856 SF	-	472,953 SF	4,903 SF	477,856 SF
OFFICE*	24,113 SF	-	14,960 SF	9,153 SF	24,113 SF
RETAIL*	12,870 SF	-	12,870 SF	-	12,870 SF
RESTAURANT*	17,223 SF	-	14,520 SF	2,703 SF	17,223 SF
CLASSROOM	3,249 SF	-	-	3,249 SF	3,249 SF
POOL & OUTDOOR AMENITY	18,800 SF	-	18,800 SF	-	18,800 SF
STRUCTURED PARKING	115,000 SF	-	115,000 SF	-	115,000 SF
CHURCH/SOCIAL HALL/KITCHEN (EXISTING)****	18,306 SF	-	-	18,306 SF	18,306 SF
OFFICE (EXISTING)*,****	3,345 SF	-	-	3,345 SF	3,345 SF
TOTAL	690,762 SF	-	649,103 SF	41,659 SF	690,762 SF

PAD NOTES:

* THESE COMMERCIAL USES ARE NOT LIMITED BY THE PAD AND MAY BE CONFIGURED DIFFERENTLY.

** THESE AREAS INCLUDE HARDSCAPE PEDESTRIAN AREAS AND POOL.

*** THESE SITES ARE NOT LIMITED BY THE PAD AND MAY BE CONFIGURED DIFFERENTLY.

**** EXISTING AREAS WERE PROVIDED BY MARICOPA COUNTY ASSESSOR'S OFFICE.

The Maxwell Tempe, LLC

712 S College Avenue
Tempe, AZ 85281

Gensler

201 East Washington St
Suite 750
Phoenix, AZ 85004
United States

Tel: 602.523.4900
Fax: 602.523.4949

Date	Description
02.22.16	Amended Site Plan Submittal
03.14.16	Amended Site Plan Submittal

Seal/Signature



Project Name

Newman Center / The Maxwell on College

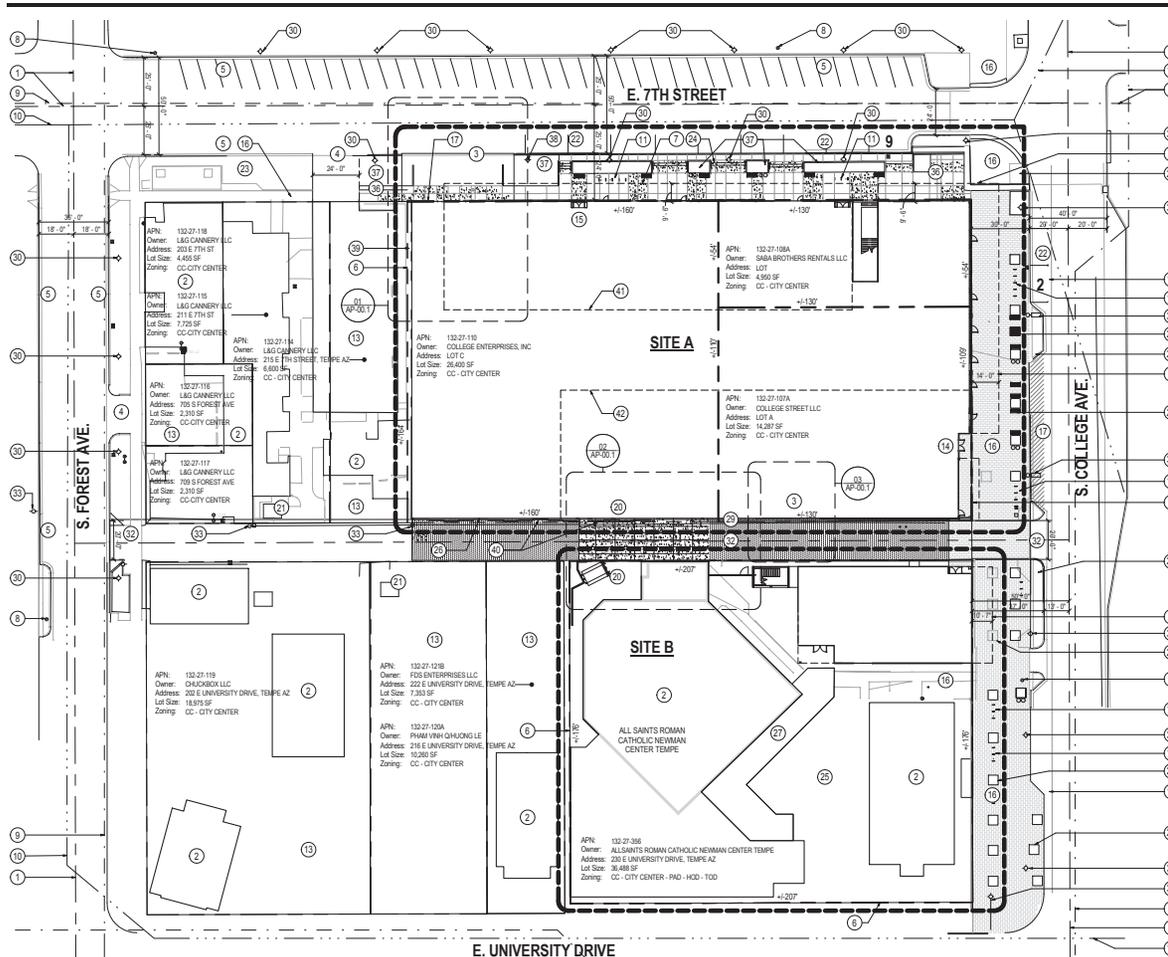
Project Number
57,6119.000

Description
PAD SITE DATA

Scale

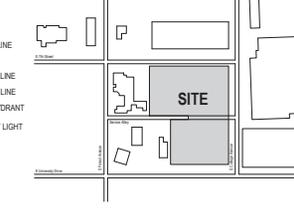
PAD-03

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- 1 STREET CENTERLINE
- 2 EXISTING BUILDING TO REMAIN
- 3 VEHICULAR DRIVEWAY ENTRY
- 4 EXISTING DRIVEWAY
- 5 EXISTING ON-STREET PARKING
- 6 PROPERTY LINE
- 7 BUILDING OVERHANG
- 8 EXISTING FIRE HYDRANT
- 9 EXISTING WATER LINE
- 10 EXISTING SEWER LINE
- 11 BIKE PARKING
- 12 EXISTING BICYCLE PARKING
- 13 EXISTING PARKING LOT
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- 15 NORTH TOWER ENTRANCE
- 16 EXISTING SIDEWALK
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- 21 EXISTING REFUSE ENCLOSURE
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- 23 EXISTING PLANTER
- 24 SEATING AREA
- 25 EXISTING COURTYARD
- 26 ELECTRICAL EQUIPMENT TO BE ACCESSED FROM ALLEY
- 27 EXISTING ARCADE
- 28 ACCESSIBLE PATH
- 29 GAS METER
- 30 EXISTING STREET LIGHT
- 31 EXISTING TRAFFIC LIGHT
- 32 EXISTING ALLEY
- 33 EXISTING UTILITY POLE
- 34 NEW UTILITY POLE
- 35 NEW STREET LIGHT
- 36 NEW SIDEWALK
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- 38 RELOCATED STREET LIGHT
- 39 DECOMPOSED GRANITE
- 40 SLIDING ACCESS GATES
- 41 NORTH TOWER ABOVE
- 42 SOUTH TOWER ABOVE

- SITE PLAN LEGEND**
- PROPERTY LINE
 - STREET CENTERLINE
 - PROJECT LIMITS
 - - - EXISTING WATER LINE
 - - - EXISTING SEWER LINE
 - ⊕ EXISTING FIRE HYDRANT
 - ⊕ EXISTING STREET LIGHT
 - STREET LIGHT



16 SITE PLAN
SCALE: 1" = 30'-0"

04 VICINITY MAP
SCALE: NTS

OWNER:	COLLEGE STREET, L.L.C.	COLLEGE ENTERPRISES, INC.	SABA BROTHERS RENTALS, LLC	ALL SAINTS ROMAN CATHOLIC NEWMAN CENTER TEMPE
713 S. COLLEGE AVE TEMPE, AZ 85281	704 S. COLLEGE AVE TEMPE, AZ 85281	704 S. COLLEGE AVE TEMPE, AZ 85281	230 E. UNIVERSITY DR TEMPE, AZ 85281	
CONTACT:	THE MAX, MAXWELL TEMPE LLC	ALL SAINTS ROMAN CATHOLIC NEWMAN CENTER	TREVOR BARGER	
TONY J. WALL	1204 N. VAN PASO DEL SUR SUITE 515 BOX 261	1626 N. SCOTTSDALE RD. ANTES BUILDING, SUITE E	SCOTTSDALE, AZ 85259-3764	
SITE:	SITE A	SITE B		
JURISDICTION:	CITY OF TEMPE	CITY OF TEMPE		
ZONING:	CC-CITY CENTER	CC-CENTER CENTER		
OVERLAY DISTRICT:	TOD - CORRIDOR	TOD - CORRIDOR, HOD		
PARCEL SIZE:	14,637 SF / 0.34 AC	17,670 SF NEW + 21,650 SF EXISTING = 39,321 SF		
PROPOSED BUILDING AREA:	531,478 SF			
PROPOSED USES:	MIXED USE	MIXED USE		
RESTAURANT*	13,200 SF (2%)	2,310 SF (6%)		
OFFICE*	13,600 SF (2%)	7,823 SF (21%)		
RETAIL*	11,700 SF (2%)	N/A		
CLASSROOM	N/A	2,777 SF (8%)		
RESIDENTIAL	459,178 SF (74%)	4,760 SF (13%)		
POOL & OUTDOOR AMENITY	18,800 SF (3%)	N/A		
STRUCTURED PARKING	115,000 SF (19%)	N/A		
CHURCH (EXISTING)**	N/A	19,308 SF (49%)		
OFFICE (EXISTING)**	N/A	3,345 SF (9%)		
BUILDING HEIGHT:	23'	45' (FOR THE NEW CONSTRUCTION)		
LOT COVERAGE:	99% (45,307 SF)	12% (26,221 SF)		
LANDSCAPE AREA**	1% (330 SF GROUND LEVEL)	18% (6,500 SF GROUND LEVEL)		
TYPE OF CONSTRUCTION:	TYPE IIA	TYPE IIB		
DENSITY:	273 DWELLING UNITS / ACRE	1.2 DWELLING UNITS / ACRE		
TOTAL UNITS:	286 DWELLING UNITS	1 DWELLING UNIT		
FRONT SETBACK:	0'-0" (EAST) 15' BUILDING ENCRoACHMENT INTO ROW	0'-0" (SOUTH)		
SIDE SETBACK:	0'-0" (SOUTH)	0'-0" (WEST)		
STREET SIDE SETBACK:	0'-0" (NORTH) 12' BUILDING ENCRoACHMENT INTO ROW	0'-0" (EAST) 15' BUILDING ENCRoACHMENT INTO ROW		
REAR SETBACK:	0'-0" (WEST)	0'-0" (NORTH)		
VEHICLE PARKING:				
RESIDENTIAL USE:				
1 BEDROOM (19 UNITS)	9.50 (0.9BED)	0		
2 BEDROOM (154 UNITS)	67.76 (0.22BED)	0		
3 BEDROOM (1 UNIT)	0	0 (0.0BED - LIVING/WORK RECTORY)		
4 BEDROOM (113 UNITS)	0 (0.0BED)	0		
GUEST	28.60 (0.1UNIT)	0 (0.0UNIT)		
TOTAL RESIDENTIAL	126.86	0 (0.0UNIT)		
COMMERCIAL OFFICE	67 (4000, 0.1500)	29.27 (1.050)		
RESTAURANT, RETAIL	0	0		
CLASSROOM USE	0	7.93 (1.050)		
CHURCH USE (EXISTING)	0	0 (0.000 - PROVIDED OFF SITE ON WEEKENDS)		
OFFICE (EXISTING)	0	0 (0.050 - CONVERTED TO STORAGE)		
TOTAL VEHICLE PARKING	173	28		
TOTAL VEHICLE PARKING PROVIDED:	209 (INCLUDING 28 FROM SITE B) (98 SPACES)	INCLUDED ON SITE A		
TOTAL BICYCLE PARKING ON STREET:	11	0		
BICYCLE PARKING:				
RESIDENTIAL USE:				
1 BEDROOM (19 UNITS)	19.00 (1.0BED)	0		
2 BEDROOM (154 UNITS)	308.00 (1.0BED)	0		
3 BEDROOM (1 UNIT)	0	3 (1.0BED)		
4 BEDROOM (113 UNITS)	452.00 (1.0BED)	0		
GUEST	0 (0.0UNIT)	0 (0.0UNIT - ENHANCED RESIDENT PARKING PROVIDED INSTEAD)		
TOTAL RESIDENTIAL	779	3		
OFFICE USE	4 (18000, 4 MIN)	4 (18000, 4 MIN COMBINED W/ SITE A)		
RESTAURANT USE	26.40 (1.050)	5.41 (1.050)		
RETAIL USE	4 (17500, 4 MIN)	0		
CLASSROOM USE	0	2.17 (1.050)		
CHURCH USE (EXISTING)	0	12.20 (1.050)		
OFFICE (EXISTING)	0	4 (18000, 4 MIN COMBINED W/ SITE A)		
TOTAL BICYCLE PARKING PROVIDED:	609	31		
TOTAL BICYCLE PARKING ON STREET:	779	31		
FIRE PROTECTION:	BUILDING EQUIPPED WITH AUTOMATIC EXTINGUISHING SYSTEM	BUILDING EQUIPPED WITH AUTOMATIC EXTINGUISHING SYSTEM		
OCCUPANCY CLASSIFICATIONS:				
GROUP A-2	RESTAURANT	RESTAURANT		
GROUP A-3	MEETING ROOMS, AMENITY SPACES, POOL DECK	CLASSROOMS, MEETING ROOMS, SANCTUARY (EXISTING)		
GROUP B	LEASING OFFICE	CLASSROOM AND MINISTRY OFFICES		
GROUP M	RETAIL SPACES	N/A		
GROUP R-2	RESIDENTIAL	RESIDENTIAL		
GROUP S-2	ENCLOSED PARKING	N/A		

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** THESE AREAS INCLUDE HARDCAPE PEDESTRIAN AREAS AND POOL.
*** EXISTING AREAS WERE PROVIDED BY MARICOPA COUNTY ASSESSOR'S OFFICE.

02 PROJECT DATA
SCALE: NTS

The Maxwell Tempe, LLC
712 S College Avenue
Tempe, AZ 85281

Gensler

201 East Washington St
Suite 730
Phoenix, AZ 85004
United States

Date	Description
02.22.18	Amended Site Plan Submittal
03.14.18	Amended Site Plan Submittal

Seal/Signature
NOT FOR CONSTRUCTION



Project Name
Newman Center / The Maxwell on College

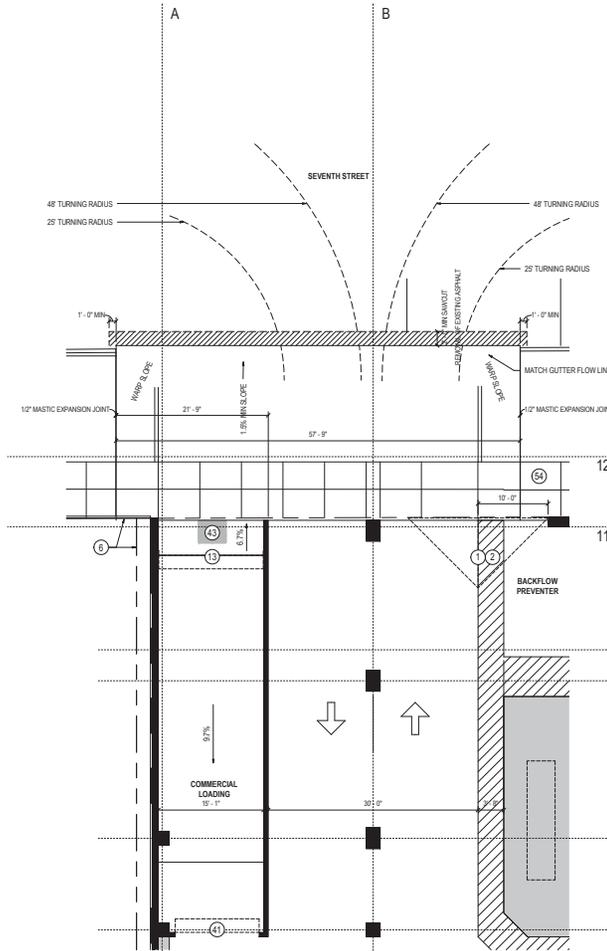
Project Number
57.6119.000

Description
SITE PLAN

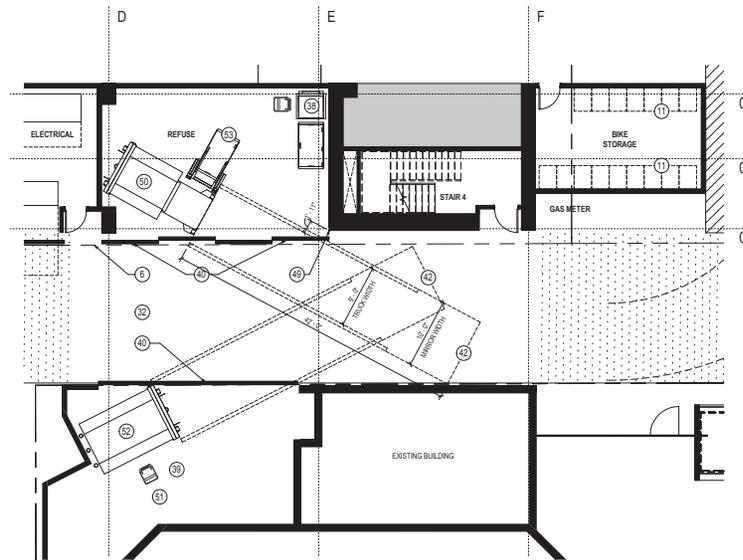
Scale
As indicated

AP-00

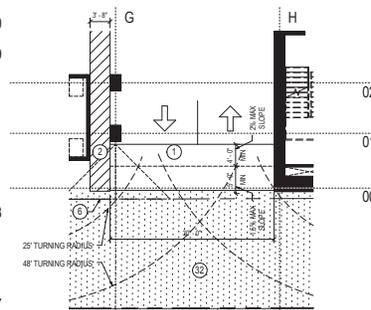
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01 NORTH PARKING GARAGE ENTRANCE
SCALE: 1" = 5'-0"



02 SOUTH TRASH COLLECTION
SCALE: 1" = 5'-0"



03 SOUTH PARKING GARAGE ENTRANCE
SCALE: 3/32" = 1'-0"

- LEGEND**
- ① 10' X 10' SIGHT VISIBILITY TRIANGLE
 - ② PEDESTRIAN PATH TO PUBLIC WAY
 - ③ VEHICULAR DRIVEWAY ENTRY
 - ④ VEHICLE SCREEN WALL
 - ⑤ VEHICLE TURN-AROUND
 - ⑥ PROPERTY LINE
 - ⑦ PARKING RAMP
 - ⑧ SECTION CUT THROUGH PARKING RAMP
 - ⑨ EDGE OF PARKING DECK
 - ⑩ BICYCLE WALL MOUNT
 - ⑪ BICYCLE PARKING
 - ⑫ EXISTING BICYCLE PARKING
 - ⑬ OVERHEAD SECURITY DOOR
 - ⑭ SOUTH TOWER ENTRANCE
 - ⑮ NORTH TOWER ENTRANCE
 - ⑯ PARKING GATE WITH INTERCOM
 - ⑰ COMMERCIAL LOADING ZONE
 - ⑱ RESIDENTIAL LOADING ZONE
 - ⑲ VAN ACCESSIBLE PARKING SPACE
 - ⑳ ACCESSIBLE PARKING SPACE
 - ㉑ OPEN TO ABOVE
 - ㉒ OPEN TO BELOW
 - ㉓ PEDESTRIAN BRIDGE
 - ㉔ GUARDRAIL
 - ㉕ FULL HEIGHT SECURITY GATE
 - ㉖ ELECTRICAL EQUIPMENT
 - ㉗ EXIT DISCHARGE
 - ㉘ ACCESSIBLE PATH
 - ㉙ LINE OF BUILDING ABOVE
 - ㉚ PUBLIC SEATING
 - ㉛ ON-STREET PARKING
 - ㉜ EXISTING ALLEY
 - ㉝ POOL
 - ㉞ POOL DECK
 - ㉟ PLANTER WITH INTEGRAL SEATING
 - ㊱ PLANTER
 - ㊲ PLAZA DECK
 - ㊳ REFUSE & RECYCLE COLLECTION
 - ㊴ RECYCLE COLLECTION
 - ㊵ SLIDING ACCESS GATES
 - ㊶ OVERHEAD COILING DOOR
 - ㊷ SOLID WASTE TRUCK CLEARANCE
 - ㊸ STORM WATER OVERFLOW GRATING
 - ㊹ STORM WATER VAULT
 - ㊺ BACKFLOW PREVENTION DEVICE
 - ㊻ ACCESSIBLE RAMP TO POOL DECK
 - ㊼ FIRE DEPARTMENT CONNECTION
 - ㊽ BI-DIRECTIONAL AMPLIFIER LOCATION
 - ㊾ STEEL PLATE CORNER GUARD
 - ㊿ 15 CY REFUSE COMPACTOR CONTAINER
 - ① 66 GALLON TOTES TIPPER
 - ② 14 CY RECYCLING CONTAINER
 - ③ 2 CY REFUSE TIPPER
 - ④ SIDEWALK
 - ⊕ SPOT FLOOR ELEVATION

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Date	Description
02.22.16	Amended Site Plan Submittal
03.14.16	Amended Site Plan Submittal

Seal/Signature
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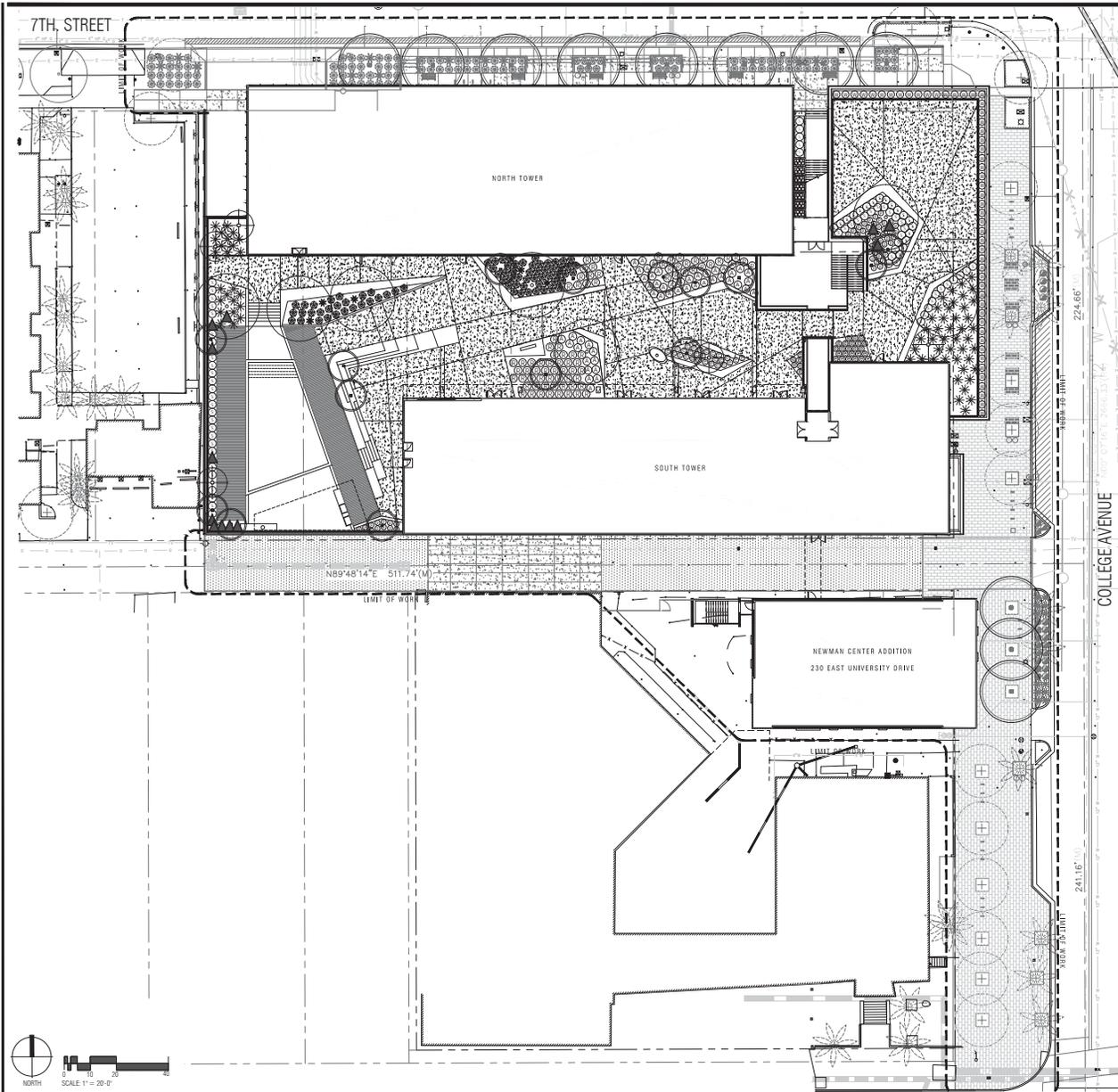
Project Number
57.6119.000

Description
ENLARGED SITE PLANS

Scale
As indicated

AP-00.1

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PLANT LEGEND - PODIUM LEVEL 4

SYMBOL	TYPE	SIZE	QTY	HEIGHT x WIDTH	CALIPER
TREES					
○	<i>Cercidium praecox</i> NZP	48" Box	7	10.0-12.0	10.0-12.0 3.0-4.0
○	Palm Tree	24" Box	15	3.0-4.0	2.0-3.0 0.75-1.0
○	<i>Sophora secundiflora</i>				
○	Texas Mountain Laurel				
SHRUBS					
⊕	<i>Cordia alliodora</i>	5 Gallon	5		
⊕	Teak Olive	3 Gallon	58		
⊕	<i>Portulacaria afra</i>				
⊕	Elephant's Foot	5 Gallon	77		
⊕	<i>Ravala bretoniana</i>				
⊕	Bauhinia				
ACCENTS					
⊕	<i>Agave bromelioides</i>	5 Gallon	50		
⊕	Agave Smooth	5 Gallon	75		
⊕	<i>Alcazabal</i>	5 Gallon	75		
⊕	Yellow Brooming Aloe	5 Gallon	68		
⊕	<i>Mulberry</i>	1 Gallon	68		
⊕	<i>Quercus laevis</i>	15 Gallon	14		
⊕	Indian Fig Prickly Pear	5 Gallon	73		
⊕	<i>Podocarpus macrocarpus</i>				
⊕	Sage Palm	5 Gallon	5		
⊕	<i>Yucca alopecuroides</i>				
⊕	Twisted Leaf Yucca				
GROUNDCOVERS/VINES					
○	<i>Sotoleros</i>	1 Gallon	53		
○	Purple Heart Plant				
○	<i>Wedelia trilobata</i>	1 Gallon	53		
○	Yellow Dot				

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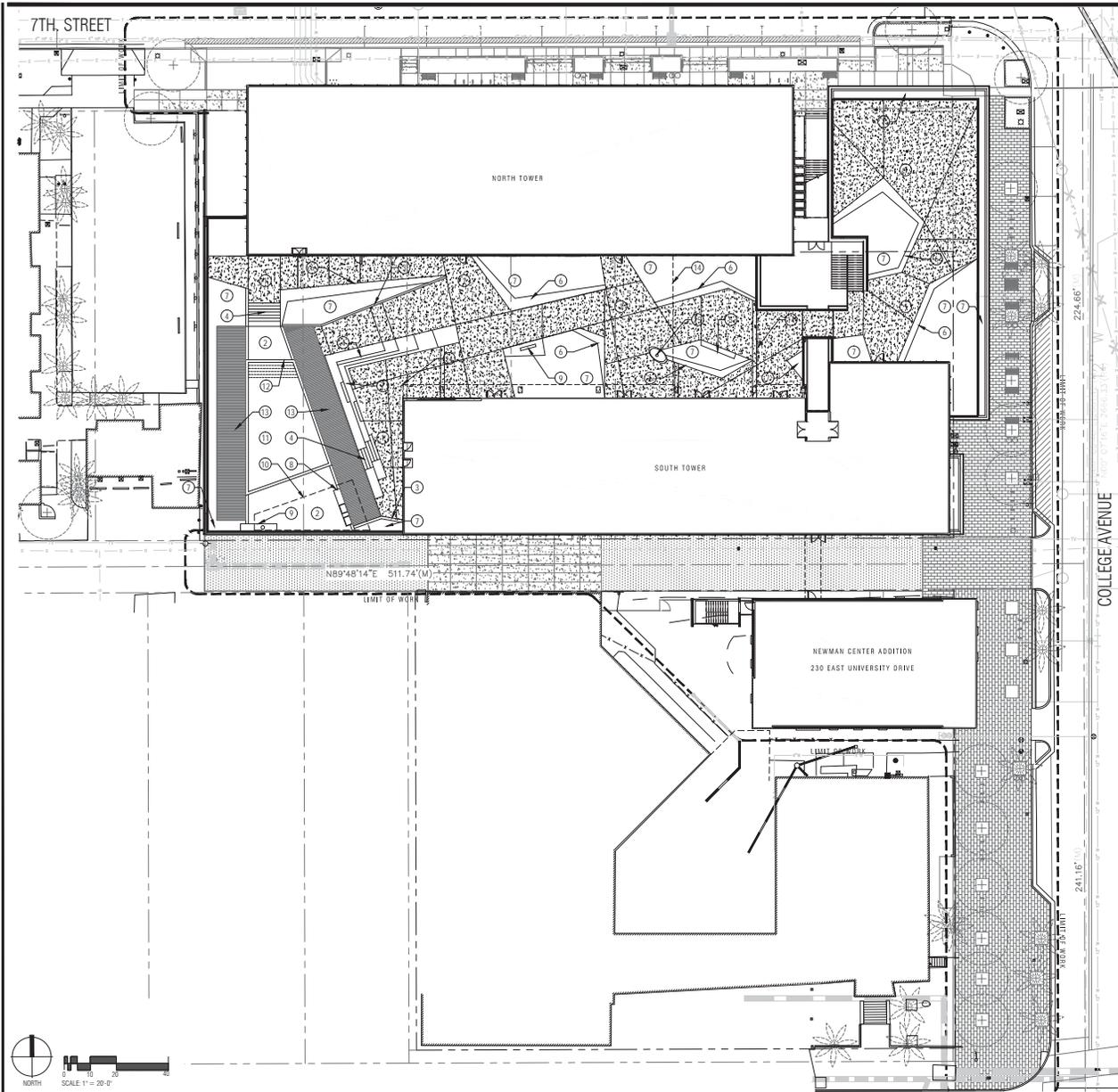
Project Number
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Description
 PLANTING PLAN
 PODIUM LEVEL 4

Scale

L2.02

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HARDSCAPE KEY NOTES:

DESCRIPTION	DETAIL SHEET
1 CIP CONCRETE PAVING TYPE 'A'	
2 CIP CONCRETE PAVING TYPE 'B'	
3 CIP CONCRETE HEADER	
4 CIP CONCRETE STAIRS	
5 CIP CONCRETE BENCH TYPE 'A'	
6 CIP CONCRETE BENCH TYPE 'B'	
7 PLANTING AREA	
8 BBQ GRILL	
9 FIRE FEATURE	
10 OVERHANG SHADE STRUCTURE	
11 POOL	
12 POOL COPING	
13 WOOD COMPOSITE POOL DECK	
14 BUILDING OVERHANG	PER ARCH.
15 WATER FEATURE	

PAVING LEGEND

SYMBOL	DESCRIPTION
[Wood Composite Pool Deck Symbol]	WOOD COMPOSITE POOL DECK
[CIP Concrete Paving Type A Symbol]	CIP CONCRETE PAVING TYPE 'A'
[CIP Concrete Paving Type B Symbol]	CIP CONCRETE PAVING TYPE 'B'

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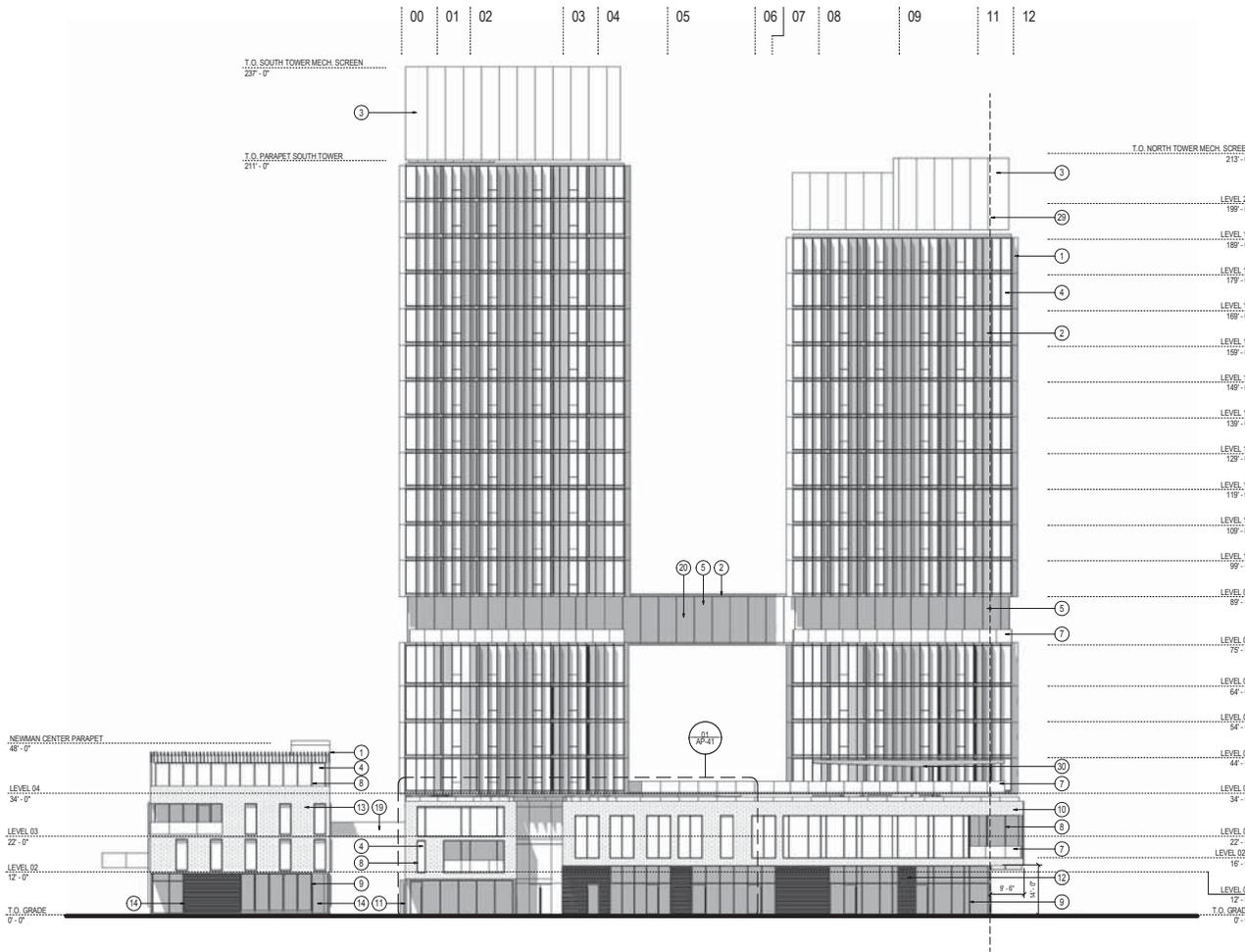
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Description
HARDSCAPE PLAN
PODIUM LEVEL 4
Scale

L1.02

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LEGEND

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- ② ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM. COLOR: CENTRIA LIGHT CHAMPAGNE
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Description

EXTERIOR EAST ELEVATIONS - NORTH & SOUTH TOWERS

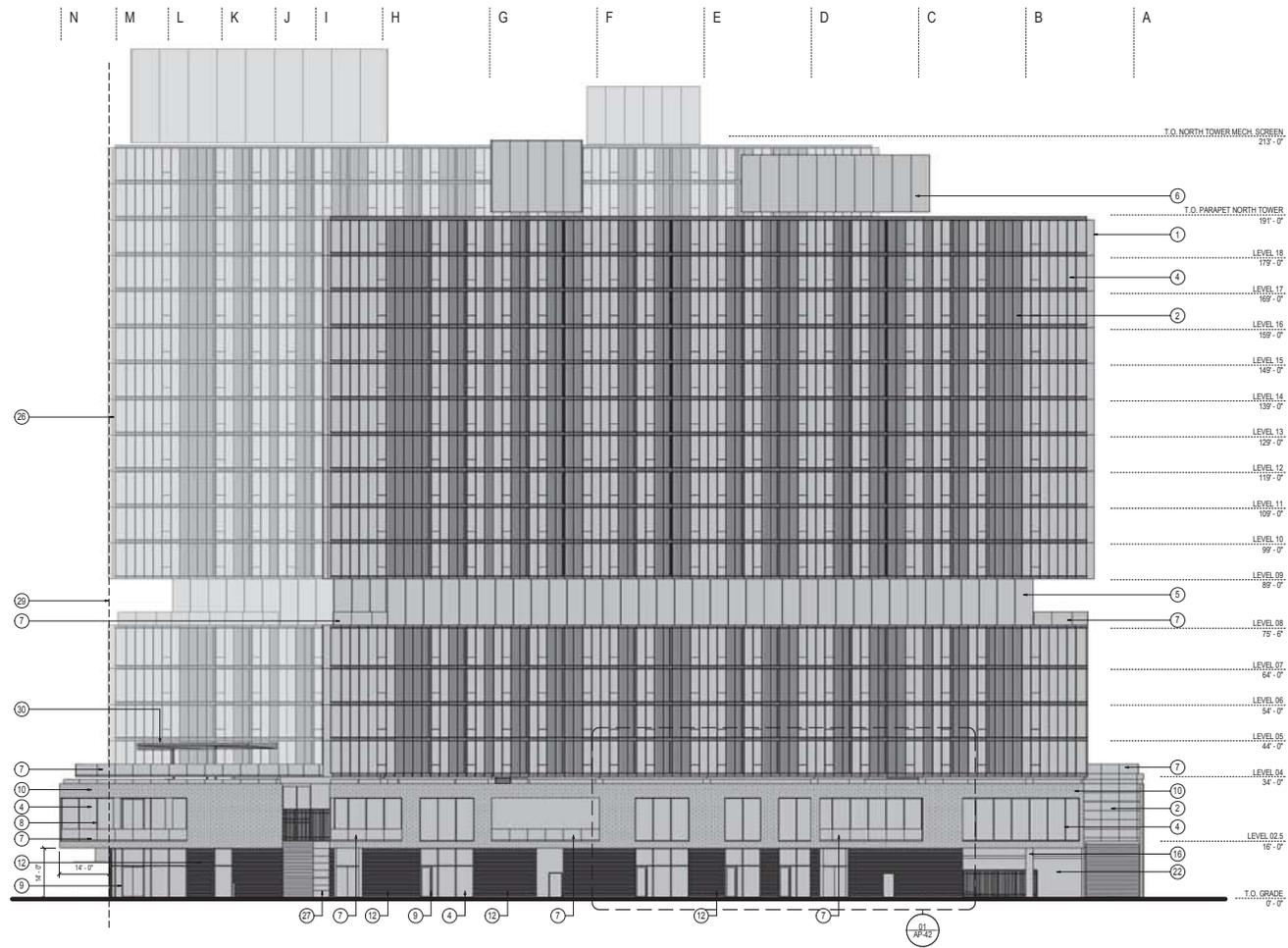
Scale

As indicated

AP-20

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01 EXTERIOR EAST ELEVATIONS - NORTH & SOUTH TOWERS
SCALE: 1/16" = 1'-0"



LEGEND

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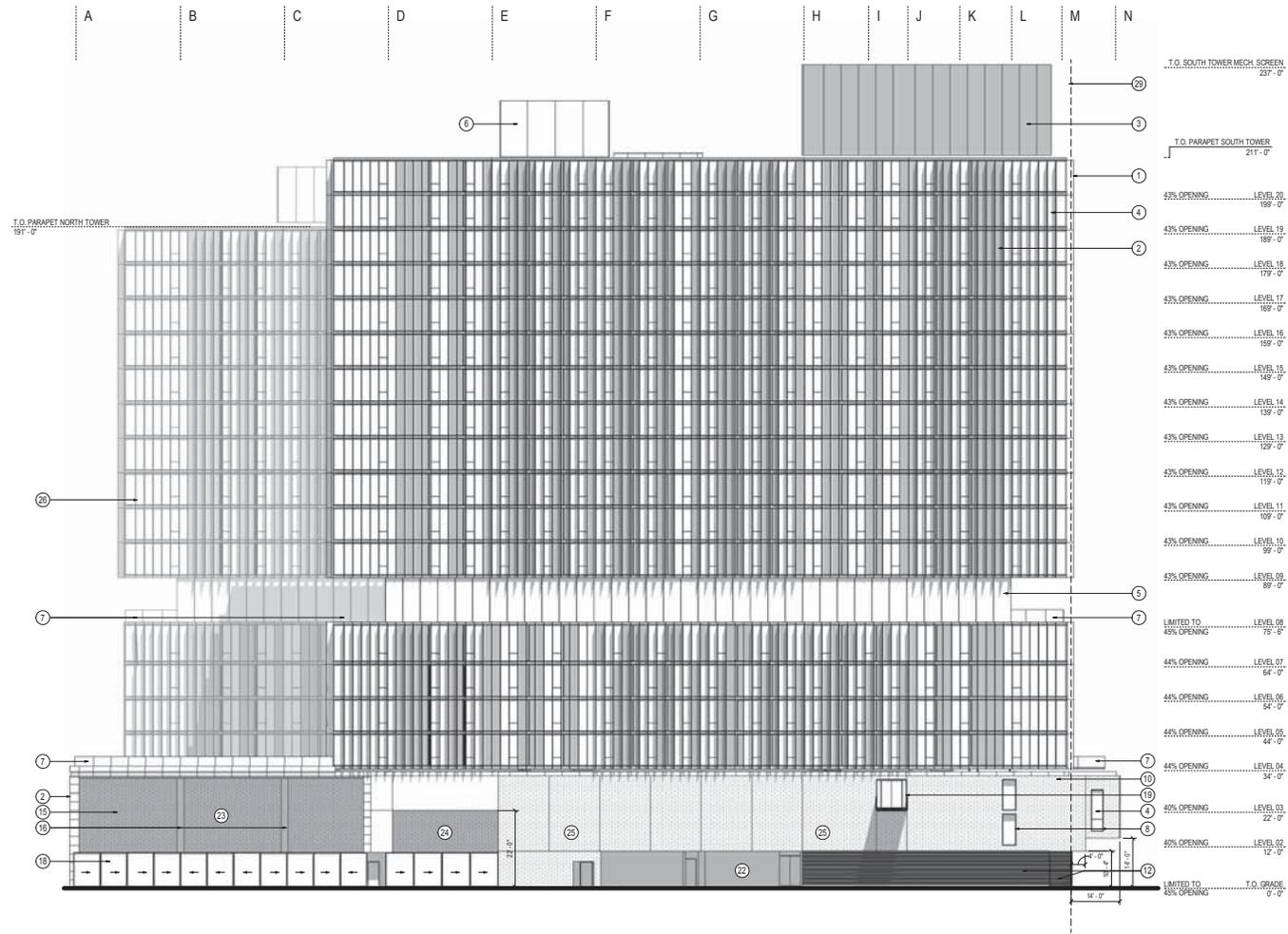
Description
EXTERIOR NORTH ELEVATION - NORTH TOWER

Scale
As indicated

AP-21

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01 EXTERIOR NORTH ELEVATION - NORTH TOWER
SCALE: 1/16" = 1'-0"



LEGEND

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Project Name
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 Project Number
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Description
EXTERIOR SOUTH ELEVATION - SOUTH TOWER

Scale
 As indicated

AP-22

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01 EXTERIOR SOUTH ELEVATION - SOUTH TOWER
 SCALE: 1/16" = 1'-0"

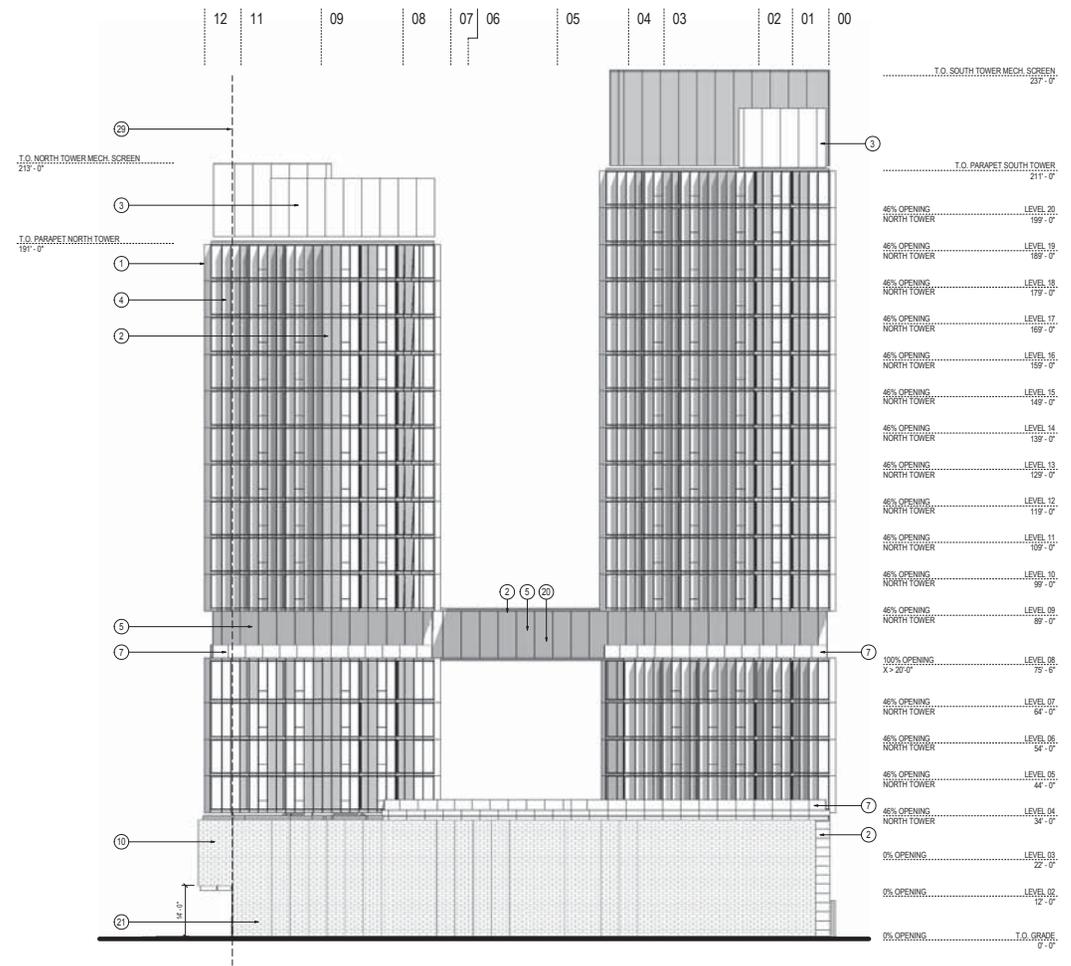
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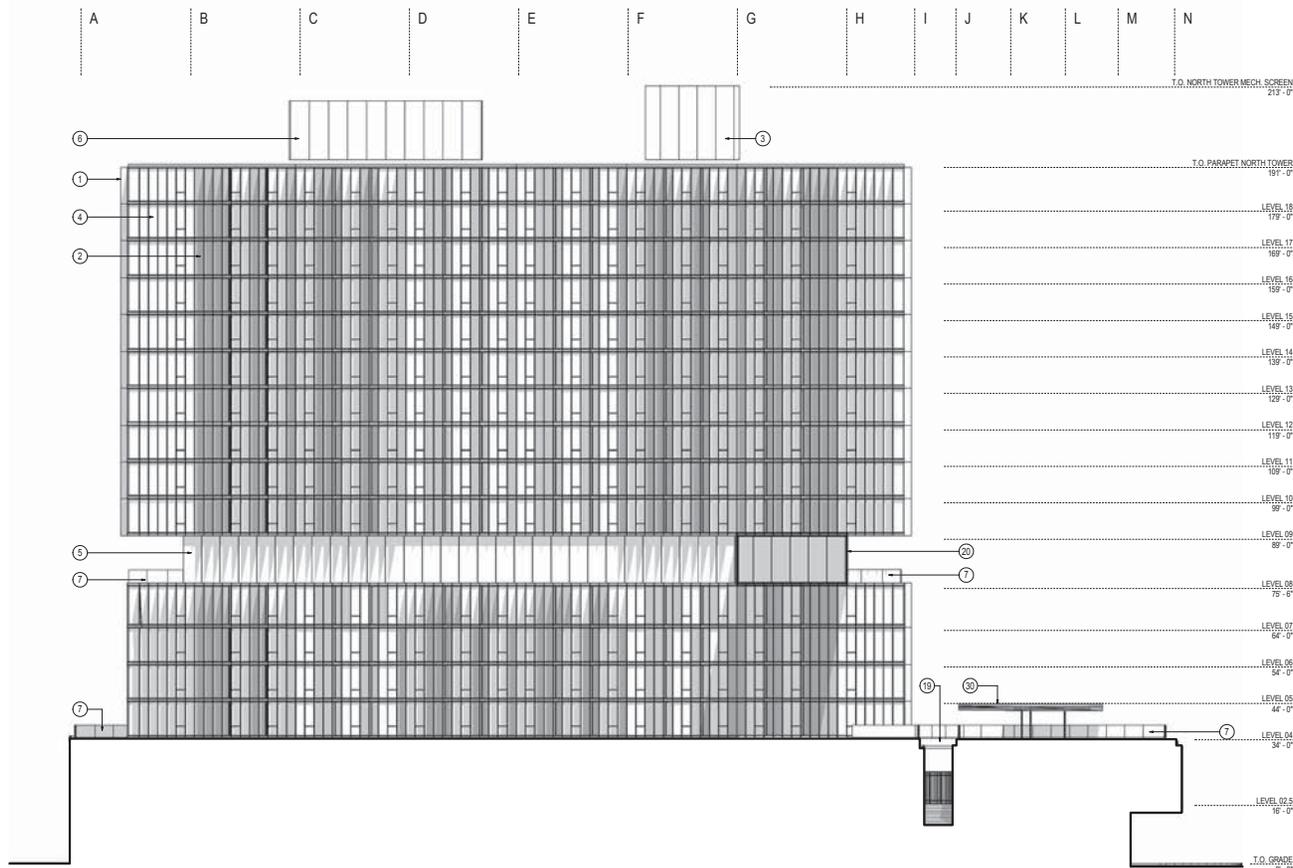
Description
EXTERIOR WEST ELEVATIONS - NORTH & SOUTH TOWERS

Scale
As indicated

AP-23

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01 EXTERIOR WEST ELEVATIONS - NORTH & SOUTH TOWERS
SCALE: 1/16" = 1'-0"



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EXTERIOR SOUTH ELEVATION - NORTH TOWER

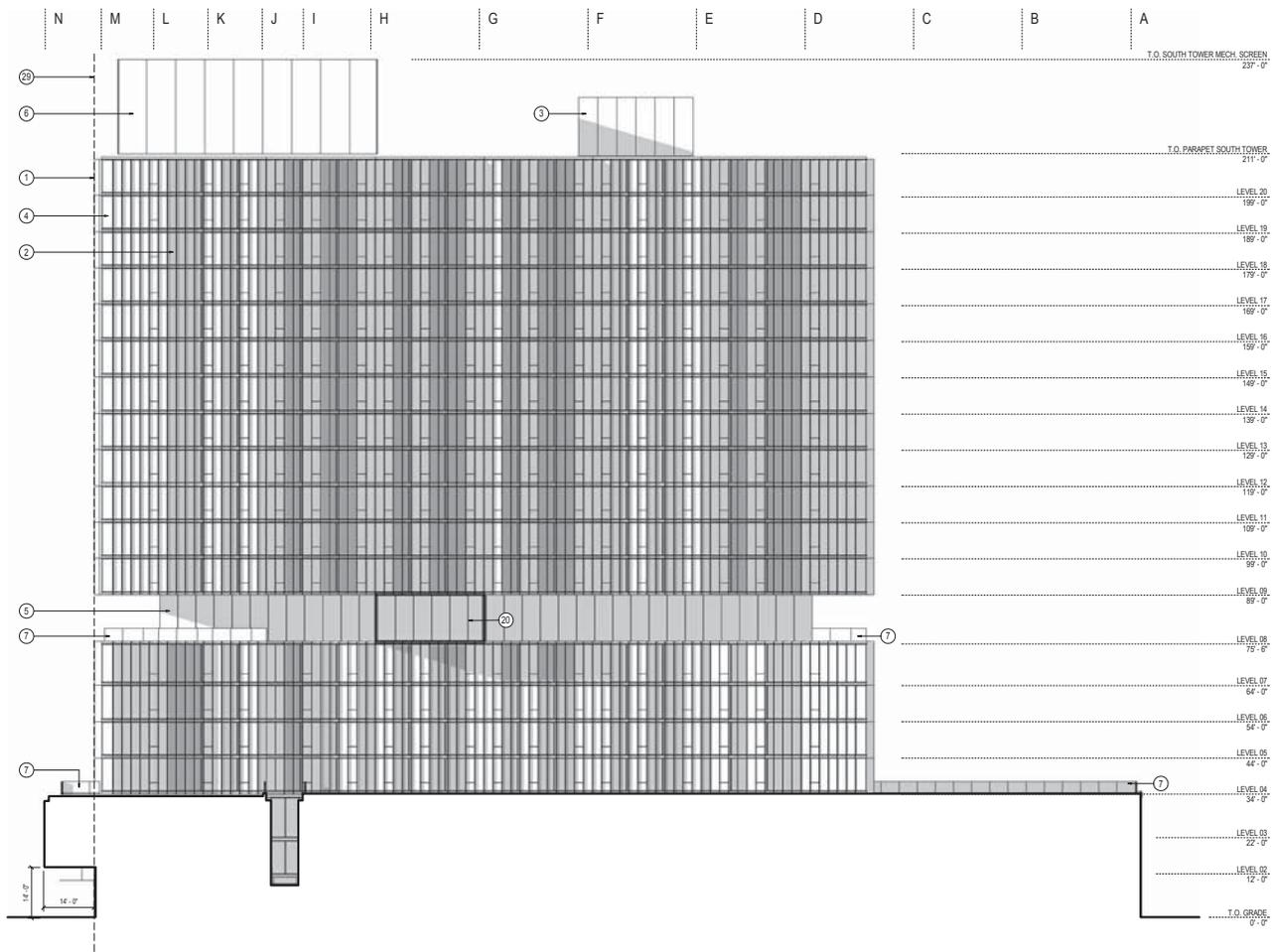
Scale

As indicated

AP-24

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01 EXTERIOR SOUTH ELEVATION - NORTH TOWER
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- ⑰ MASONRY STAINED VEHICLE SCREEN WALL. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑱ PERFORATED STEEL SLIDING EQUIPMENT ACCESS GATES. COLOR: CENTRIA SILVER GRAY
- ⑲ PEDESTRIAN BRIDGE
- ⑳ AMENITY BRIDGE
- ㉑ MASONRY STAINED. COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ㉒ VEHICULAR DRIVEWAY
- ㉓ ELECTRICAL EQUIPMENT CLEARANCE WALL OPENING
- ㉔ REFUSE COLLECTION CLEARANCE WALL OPENING
- ㉕ LOCAL ART
- ㉖ BUILDING BEYOND
- ㉗ PLANTER
- ㉘ EXISTING BUILDING
- ㉙ PROPERTY LINE
- ㉚ SHADE CANOPY

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Date	Description
02.22.16	Amended Site Plan Submittal
03.14.16	Amended Site Plan Submittal

Seal/Signature
NOT FOR CONSTRUCTION



Project Name
Newman Center / The Maxwell on College

Project Number
57.6119.000

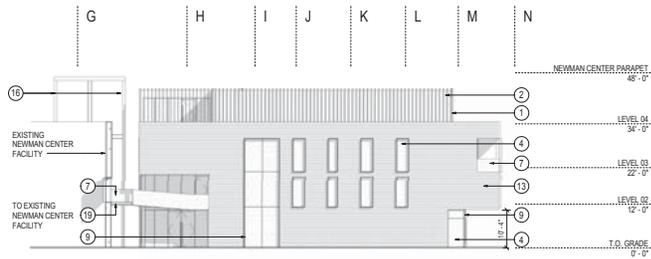
Description
EXTERIOR NORTH ELEVATION - SOUTH TOWER

Scale
As indicated

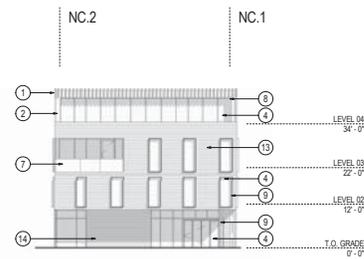
AP-25

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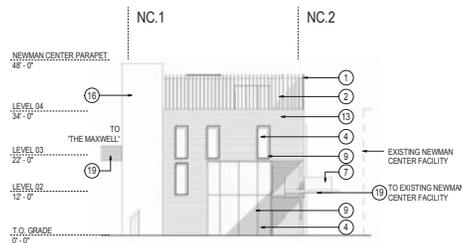
01 EXTERIOR NORTH ELEVATION - SOUTH TOWER
SCALE: 1/16" = 1'-0"



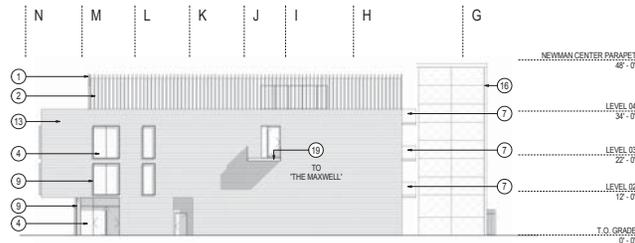
03 NEWMAN CENTER - SOUTH ELEVATION
SCALE: 1/16" = 1'-0"



01 NEWMAN CENTER - EAST ELEVATION
SCALE: 1/16" = 1'-0"



07 NEWMAN CENTER - WEST ELEVATION
SCALE: 1/16" = 1'-0"



02 NEWMAN CENTER - NORTH ELEVATION
SCALE: 1/16" = 1'-0"

LEGEND

- ① ALUMINUM EXTRUSION. COLOR: CENTRIA SILVER GRAY
- ② ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM. COLOR: CENTRIA LIGHT CHAMPAGNE
- ③ PERFORATED ALUMINUM WALL PANEL SYSTEM. COLOR: CENTRIA SILVER GRAY
- ④ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW-E COATING
- ⑤ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW IRON
- ⑥ TRANSLUCENT GLAZING SYSTEM. COLOR: FOG
- ⑦ LAMINATED GLASS GUARDRAIL. COLOR: CLEAR
- ⑧ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: CLEAR ANODIZED
- ⑨ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: DARK BRONZE
- ⑩ MASONRY. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑪ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ⑫ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46 W/ RELIEF PATTERN
- ⑬ MASONRY. COLOR: ENDICOTT RED BLEND
- ⑭ MASONRY. COLOR: ENDICOTT RED BLEND W/ RELIEF PATTERN
- ⑮ MASONRY STAINED. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑯ CAST-IN-PLACE CONCRETE. SMOOTH. COLOR: NATURAL GREY
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Description

NEWMAN CENTER EXTERIOR ELEVATIONS

Scale

As indicated

AP-26

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LEGEND

- ① ALUMINUM EXTRUSION. COLOR: CENTRIA SILVER GRAY
- ② ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM. COLOR: CENTRIA LIGHT CHAMPAGNE
- ③ PERFORATED ALUMINUM WALL PANEL SYSTEM. COLOR: CENTRIA SILVER GRAY
- ④ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW-E COATING
- ⑤ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW IRON
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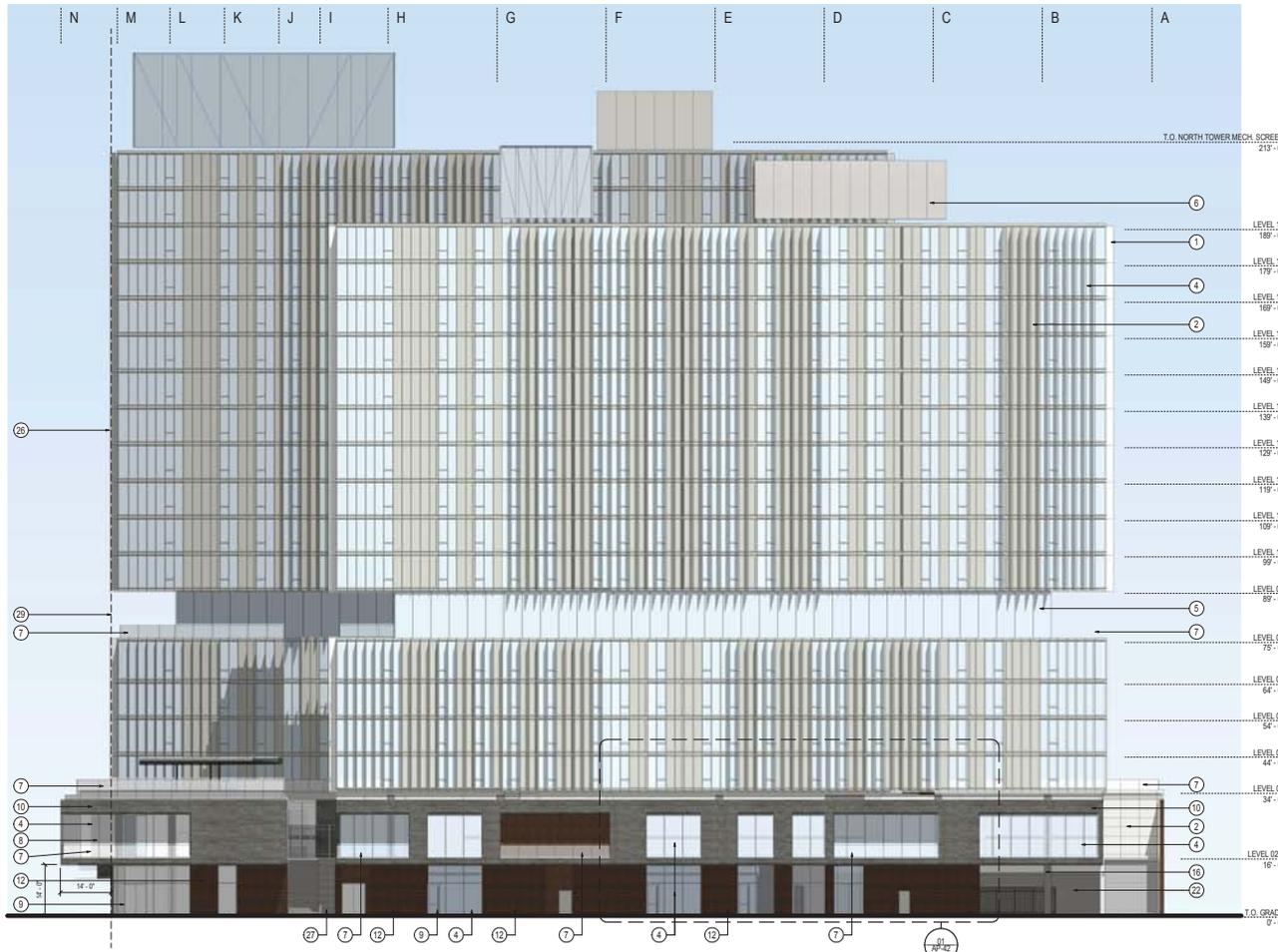
Description
 EXTERIOR EAST ELEVATIONS - NORTH & SOUTH TOWERS - COLOR

Scale
 As indicated

AP-30

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01 EXTERIOR ELEVATIONS EAST - NORTH & SOUTH TOWERS
 SCALE: 1/8" = 1'-0"



LEGEND

- ① ALUMINUM EXTRUSION. COLOR: CENTRIA SILVER GRAY
- ② ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM. COLOR: CENTRIA LIGHT CHAMPAGNE
- ③ PERFORATED ALUMINUM WALL PANEL SYSTEM. COLOR: CENTRIA SILVER GRAY
- ④ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW-E COATING
- ⑤ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW IRON
- ⑥ TRANSLUCENT GLAZING SYSTEM. COLOR: FOG
- ⑦ LAMINATED GLASS GUARDRAIL. COLOR: CLEAR
- ⑧ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: CLEAR ANODIZED
- ⑨ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: DARK BRONZE
- ⑩ MASONRY. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑪ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ⑫ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46 W/ RELIEF PATTERN
- ⑬ MASONRY. COLOR: ENDICOTT RED BLEND
- ⑭ MASONRY. COLOR: ENDICOTT RED BLEND W/ RELIEF PATTERN
- ⑮ MASONRY STAINED. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑯ CAST-IN-PLACE CONCRETE. SMOOTH. COLOR: NATURAL GREY
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- ⑱ ACCESS GATES. COLOR: CENTRIA SILVER GRAY
- ⑲ PEDESTRIAN BRIDGE
- ⑳ AMENITY BRIDGE
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- ㉒ VEHICULAR DRIVEWAY
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- ㉔ REFUSE COLLECTION CLEARANCE WALL OPENING
- ㉕ LOCAL ART
- ㉖ BUILDING BEYOND
- ㉗ PLANTER
- ㉘ EXISTING BUILDING
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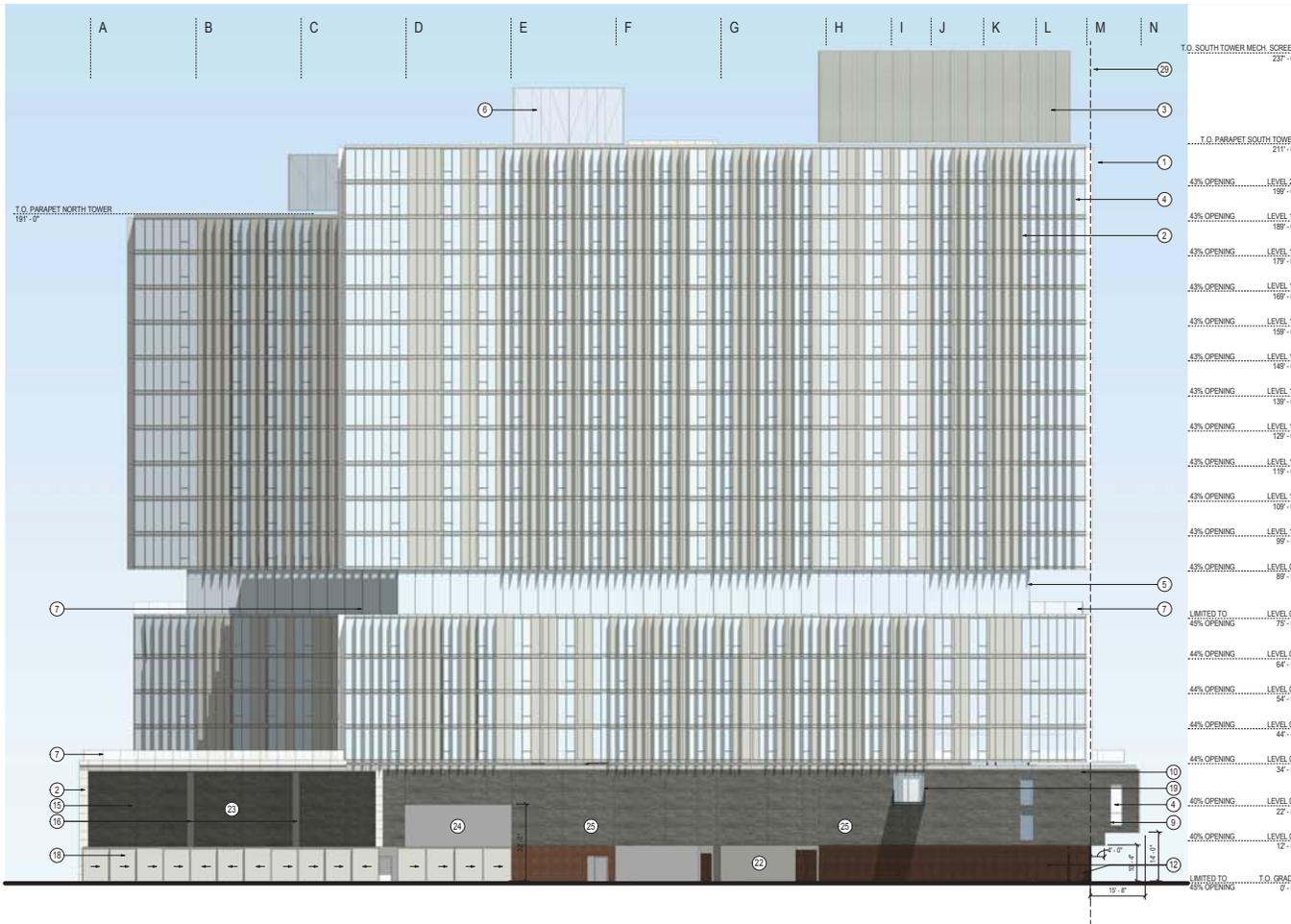
Description
 EXTERIOR NORTH ELEVATION - NORTH TOWER - COLOR

Scale
 As indicated

AP-31

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01 EXTERIOR NORTH ELEVATION - NORTH TOWER
 SCALE: 1/16" = 1'-0"



LEGEND

- ① ALUMINUM EXTRUSION, COLOR: CENTRIA SILVER GRAY
- ② ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM, COLOR: CENTRIA LIGHT CHAMPAGNE
- ③ PERFORATED ALUMINUM WALL PANEL SYSTEM, COLOR: CENTRIA SILVER GRAY
- ④ INSULATED GLAZING SYSTEM, COLOR: CLEAR, LOW-E COATING
- ⑤ INSULATED GLAZING SYSTEM, COLOR: CLEAR, LOW IRON
- ⑥ TRANSLUCENT GLAZING SYSTEM, COLOR: FOG
- ⑦ LAMINATED GLASS GUARDRAIL, COLOR: CLEAR
- ⑧ ALUMINUM WINDOW WALL FRAMING SYSTEM, COLOR: CLEAR ANODIZED
- ⑨ ALUMINUM WINDOW WALL FRAMING SYSTEM, COLOR: DARK BRONZE
- ⑩ MASONRY, COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑪ MASONRY, COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ⑫ MASONRY, COLOR: ENDICOTT MEDIUM IRONSPOT #46 W/ RELIEF PATTERN
- ⑬ MASONRY, COLOR: ENDICOTT RED BLEND
- ⑭ MASONRY, COLOR: ENDICOTT RED BLEND W/ RELIEF PATTERN
- ⑮ MASONRY STAINED, COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑯ CAST-IN-PLACE CONCRETE, SMOOTH, COLOR: NATURAL GREY
- ⑰ MASONRY STAINED VEHICLE SCREEN WALL, COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑱ ACCESS GATES, COLOR: CENTRIA SILVER GRAY
- ⑲ PEDESTRIAN BRIDGE
- ⑳ AMENITY BRIDGE
- ㉑ MASONRY STAINED, COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ㉒ VEHICULAR DRIVEWAY
- ㉓ ELECTRICAL EQUIPMENT CLEARANCE WALL OPENING
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- ㉕ LOCAL ART
- ㉖ BUILDING BEYOND
- ㉗ PLANTER
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Project Name
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Project Number
 57.6119.000

Description
 EXTERIOR SOUTH ELEVATION - SOUTH TOWER - COLOR

Scale
 As indicated

AP-32

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01 EXTERIOR SOUTH ELEVATION - SOUTH TOWER
 SCALE: 1/16" = 1'-0"

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LEGEND

- ① ALUMINUM EXTRUSION. COLOR: CENTRIA SILVER GRAY
- ② ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM. COLOR: CENTRIA LIGHT CHAMPAGNE
- ③ PERFORATED ALUMINUM WALL PANEL SYSTEM. COLOR: CENTRIA SILVER GRAY
- ④ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW-E COATING
- ⑤ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW IRON
- ⑥ TRANSLUCENT GLAZING SYSTEM. COLOR: FOG
- ⑦ LAMINATED GLASS GUARDRAIL. COLOR: CLEAR
- ⑧ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: CLEAR ANODIZED
- ⑨ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: DARK BRONZE
- ⑩ MASONRY. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑪ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ⑫ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46 W/ RELIEF PATTERN
- ⑬ MASONRY. COLOR: ENDICOTT RED BLEND
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- ⑮ MASONRY STAINED. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑯ CAST-IN-PLACE CONCRETE. SMOOTH. COLOR: NATURAL GREY
- ⑰ MASONRY STAINED VEHICLE SCREEN WALL. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑱ ACCESS GATES. COLOR: CENTRIA SILVER GRAY
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- ⑳ AMENITY BRIDGE
- ㉑ MASONRY STAINED. COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ㉒ VEHICULAR DRIVEWAY
- ㉓ ELECTRICAL EQUIPMENT CLEARANCE WALL OPENING
- ㉔ REFUSE COLLECTION CLEARANCE WALL OPENING
- ㉕ LOCAL ART
- ㉖ BUILDING BEYOND
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- ㉘ EXISTING BUILDING
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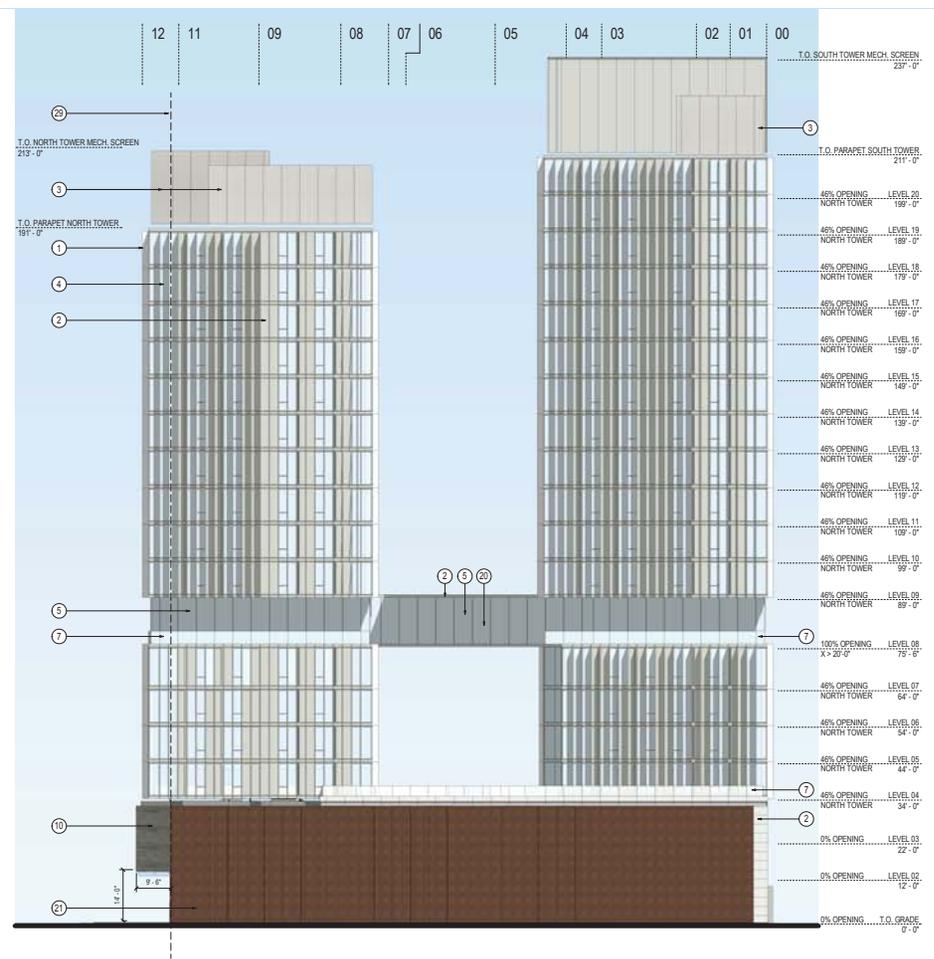
Project Number
 57.6119.000

Description
 EXTERIOR WEST ELEVATIONS - NORTH & SOUTH TOWERS - COLOR

Scale
 As indicated

AP-33

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01 EXTERIOR WEST ELEVATIONS - NORTH & SOUTH TOWERS
 SCALE: 1/16" = 1'-0"



LEGEND

- ① ALUMINUM EXTRUSION. COLOR: CENTRIA SILVER GRAY
- ② ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM. COLOR: CENTRIA LIGHT CHAMPAGNE
- ③ PERFORATED ALUMINUM WALL PANEL SYSTEM. COLOR: CENTRIA SILVER GRAY
- ④ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW-E COATING
- ⑤ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW IRON
- ⑥ TRANSLUCENT GLAZING SYSTEM. COLOR: FOG
- ⑦ LAMINATED GLASS GUARDRAIL. COLOR: CLEAR
- ⑧ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: CLEAR ANODIZED
- ⑨ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: DARK BRONZE
- ⑩ MASONRY. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑪ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ⑫ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46 W/ RELIEF PATTERN
- ⑬ MASONRY. COLOR: ENDICOTT RED BLEND
- ⑭ MASONRY. COLOR: ENDICOTT RED BLEND W/ RELIEF PATTERN
- ⑮ MASONRY STAINED. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑯ CAST-IN-PLACE CONCRETE. SMOOTH. COLOR: NATURAL GREY
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- ⑱ PERFORATED STEEL SLIDING EQUIPMENT ACCESS GATES. COLOR: CENTRIA SILVER GRAY
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Description

EXTERIOR SOUTH ELEVATION - NORTH TOWER - COLOR

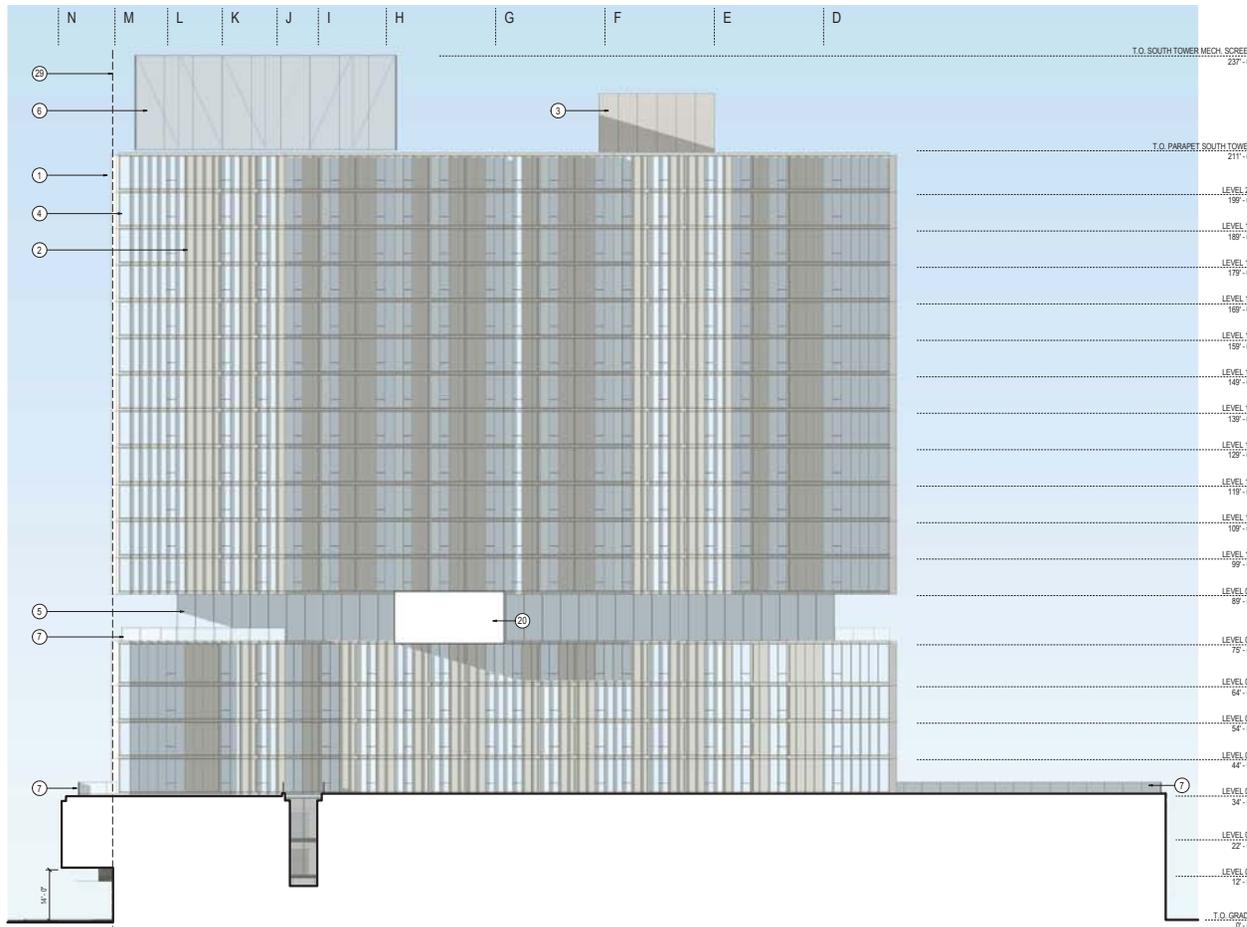
Scale

As indicated

AP-34

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01 EXTERIOR SOUTH ELEVATION - NORTH TOWER COLOR
 SCALE: 1/16" = 1'-0"



LEGEND

- ① ALUMINUM EXTRUSION. COLOR: CENTRIA SILVER GRAY
- ② ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM. COLOR: CENTRIA LIGHT CHAMPAGNE
- ③ PERFORATED ALUMINUM WALL PANEL SYSTEM. COLOR: CENTRIA SILVER GRAY
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- ㉒ VEHICULAR DRIVEWAY
- ㉓ ELECTRICAL EQUIPMENT CLEARANCE WALL OPENING
- ㉔ REFUSE COLLECTION CLEARANCE WALL OPENING
- ㉕ LOCAL ART
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Description

EXTERIOR NORTH ELEVATION - SOUTH TOWER - COLOR

Scale

As indicated

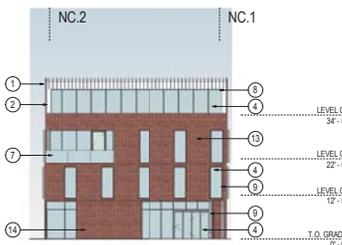
AP-35

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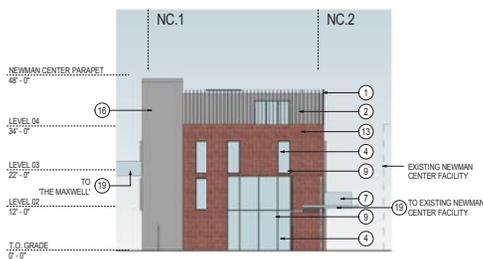
01 EXTERIOR NORTH ELEVATION - SOUTH TOWER
 SCALE: 1/16" = 1'-0"



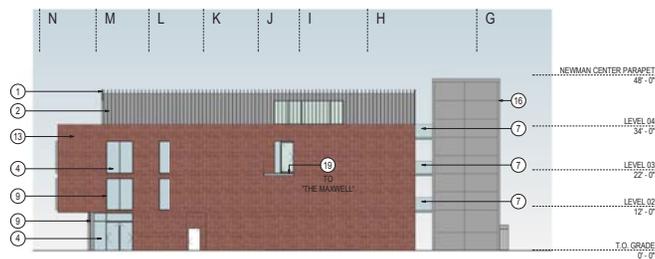
03 NEWMAN CENTER - SOUTH ELEVATION
SCALE: 1/16" = 1'-0"



01 NEWMAN CENTER - EAST ELEVATION
SCALE: 1/16" = 1'-0"



07 NEWMAN CENTER - WEST ELEVATION
SCALE: 1/16" = 1'-0"



02 NEWMAN CENTER - NORTH ELEVATION
SCALE: 1/16" = 1'-0"

LEGEND

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- ⑩ MASONRY. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑪ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ⑫ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46 W/ RELIEF PATTERN
- ⑬ MASONRY. COLOR: ENDICOTT RED BLEND
- ⑭ MASONRY. COLOR: ENDICOTT RED BLEND W/ RELIEF PATTERN
- ⑮ MASONRY STAINED. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑯ CAST-IN-PLACE CONCRETE. SMOOTH. COLOR: NATURAL GREY
- ⑰ MASONRY STAINED VEHICLE SCREEN WALL. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑱ ACCESS GATES. COLOR: CENTRIA SILVER GRAY
- ⑲ PEDESTRIAN BRIDGE
- ⑳ AMENITY BRIDGE
- ㉑ MASONRY STAINED. COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ㉒ VEHICULAR DRIVEWAY
- ㉓ ELECTRICAL EQUIPMENT CLEARANCE WALL OPENING
- ㉔ REFUSE COLLECTION CLEARANCE WALL OPENING
- ㉕ LOCAL ART
- ㉖ BUILDING BEYOND
- ㉗ PLANTER
- ㉘ EXISTING BUILDING
- ㉙ PROPERTY LINE
- ㉚ SHADE CANOPY

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Date	Description
02.22.16	Amended Site Plan Submittal
03.14.16	Amended Site Plan Submittal

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Project Name

Newman Center / The Maxwell on College

Project Number

57.6119.000

Description

NEWMAN CENTER ELEVATIONS - COLOR

Scale

As indicated

AP-36

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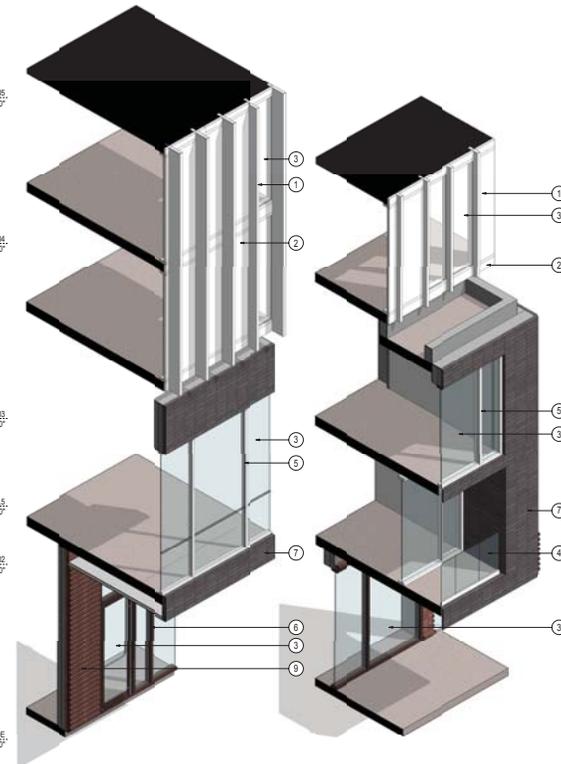
05 ENDICOTT MANGANESE IRONSPOT
SCALE: 1/2" = 1'-0"



04 ENDICOTT MEDIUM IRONSPOT #46
SCALE: 1/2" = 1'-0"



03 ENLARGED ELEVATION - BRICK
SCALE: 1/4" = 1'-0"



02 BRICK DETAIL AXONOMETRIC
SCALE: 1/4" = 1'-0"

01 BRICK DETAIL AXONOMETRIC
SCALE: 1/4" = 1'-0"

LEGEND

- ① ALUMINUM EXTRUSION. COLOR: CENTRIA SILVER GRAY
- ② ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM. COLOR: CENTRIA LIGHT CHAMPAGNE
- ③ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW-E COATING
- ④ LAMINATED GLASS GUARDRAIL. COLOR: CLEAR
- ⑤ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: CLEAR ANODIZED
- ⑥ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: DARK BRONZE
- ⑦ MASONRY. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑧ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ⑨ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46 W/ RELIEF PATTERN
- ⑩ CAST-IN-PLACE CONCRETE. SMOOTH. COLOR: NATURAL GREY

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Project Name
Newman Center / The Maxwell on College
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Description
ENLARGED ELEVATIONS & SECTIONS - BRICK DETAILING

Scale
As indicated

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LEGEND

- ① ALUMINUM EXTRUSION. COLOR: CENTRIA SILVER GRAY
- ② ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM. COLOR: CENTRIA LIGHT CHAMPAGNE
- ③ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOWE COATING
- ④ LAMINATED GLASS GUARDRAIL. COLOR: CLEAR
- ⑤ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: CLEAR ANODIZED
- ⑥ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: DARK BRONZE
- ⑦ MASONRY. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑧ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ⑨ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46 W/ RELIEF PATTERN
- ⑩ CAST-IN-PLACE CONCRETE, SMOOTH. COLOR: NATURAL GREY

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Project Name
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Project Number
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Description
ENLARGED ELEVATIONS & SECTIONS - RETAIL EAST

Scale
As indicated

AP-41

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01 ENLARGED ELEVATION - RETAIL EAST
SCALE: 1/4" = 1'-0"

LEGEND

- ① ALUMINUM EXTRUSION. COLOR: CENTRIA SILVER GRAY
- ② ALUMINUM COMPOSITE METAL WALL PANEL SYSTEM. COLOR: CENTRIA LIGHT CHAMPAGNE
- ③ INSULATED GLAZING SYSTEM. COLOR: CLEAR, LOW-E COATING
- ④ LAMINATED GLASS GUARDRAIL. COLOR: CLEAR
- ⑤ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: CLEAR ANODIZED
- ⑥ ALUMINUM WINDOW WALL FRAMING SYSTEM. COLOR: DARK BRONZE
- ⑦ MASONRY. COLOR: ENDICOTT MANGANESE IRONSPOT
- ⑧ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46
- ⑨ MASONRY. COLOR: ENDICOTT MEDIUM IRONSPOT #46 W/ RELIEF PATTERN
- ⑩ CAST-IN-PLACE CONCRETE. SMOOTH. COLOR: NATURAL GREY

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Project Name
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Project Number
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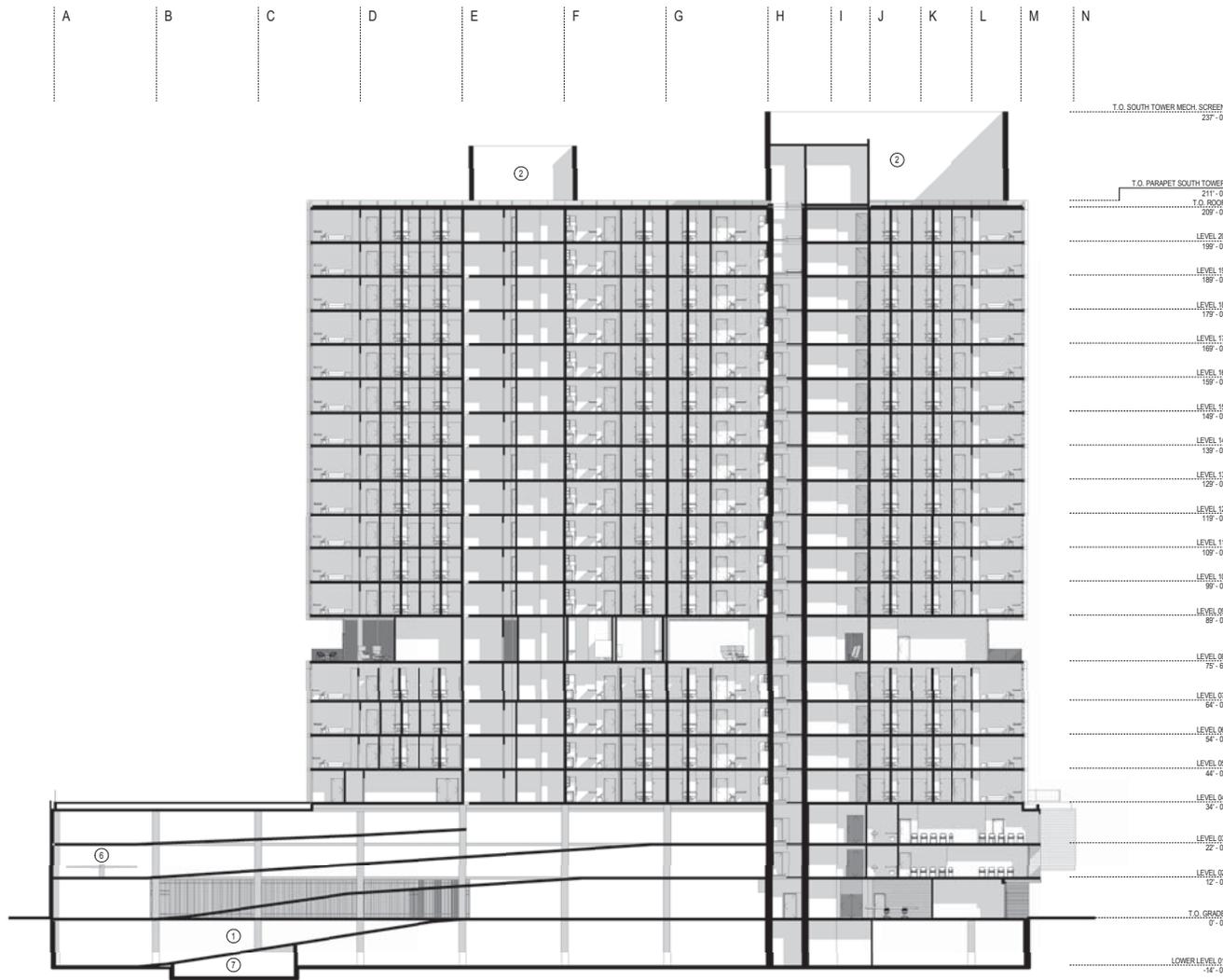
Description
ENLARGED ELEVATIONS - RETAIL NORTH

Scale
As indicated

AP-42

01 ENLARGED ELEVATIONS - RETAIL NORTH
SCALE: 1/4" = 1'-0"

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- LEGEND
- ① PARKING RAMP
 - ② MECHANICAL PENTHOUSE
 - ③ PEDESTRIAN BRIDGE
 - ④ AMENITY BRIDGE
 - ⑤ SLIDING EQUIPMENT ACCESS GATES
 - ⑥ PARKING GARAGE GATES
 - ⑦ STORM WATER VAULT
 - ⑧ COMMERCIAL LOADING AREA

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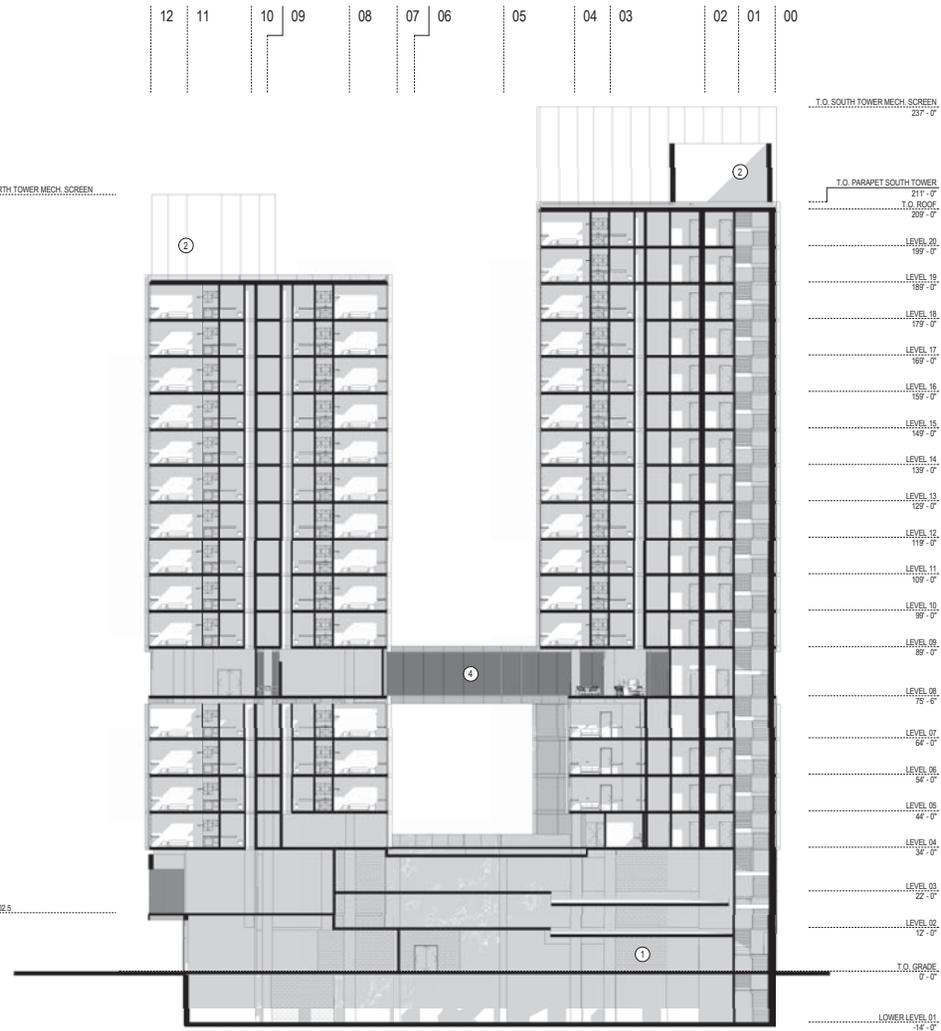
Description
BUILDING SECTIONS

Scale
As indicated

AP-50

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01 EAST WEST SECTION - 1
SCALE: 1/16" = 1'-0"



- LEGEND
- ① PARKING RAMP
 - ② MECHANICAL PENTHOUSE
 - ③ PEDESTRIAN BRIDGE
 - ④ AMENITY BRIDGE
 - ⑤ SLIDING EQUIPMENT ACCESS GATES
 - ⑥ PARKING GARAGE GATES
 - ⑦ STORM WATER VAULT
 - ⑧ COMMERCIAL LOADING AREA

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Project Name

Newman Center / The
Maxwell on College

Project Number

57.6119.000

Description

BUILDING SECTIONS

Scale

As indicated

AP-51

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02 NORTH SOUTH SECTION - 2
SCALE: 1/16" = 1'-0"



- LEGEND
- ① PARKING RAMP
 - ② MECHANICAL PENTHOUSE
 - ③ PEDESTRIAN BRIDGE
 - ④ AMENITY BRIDGE
 - ⑤ SLIDING EQUIPMENT ACCESS GATES
 - ⑥ PARKING GARAGE GATES
 - ⑦ STORM WATER VAULT
 - ⑧ COMMERCIAL LOADING AREA

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**Newman Center / The
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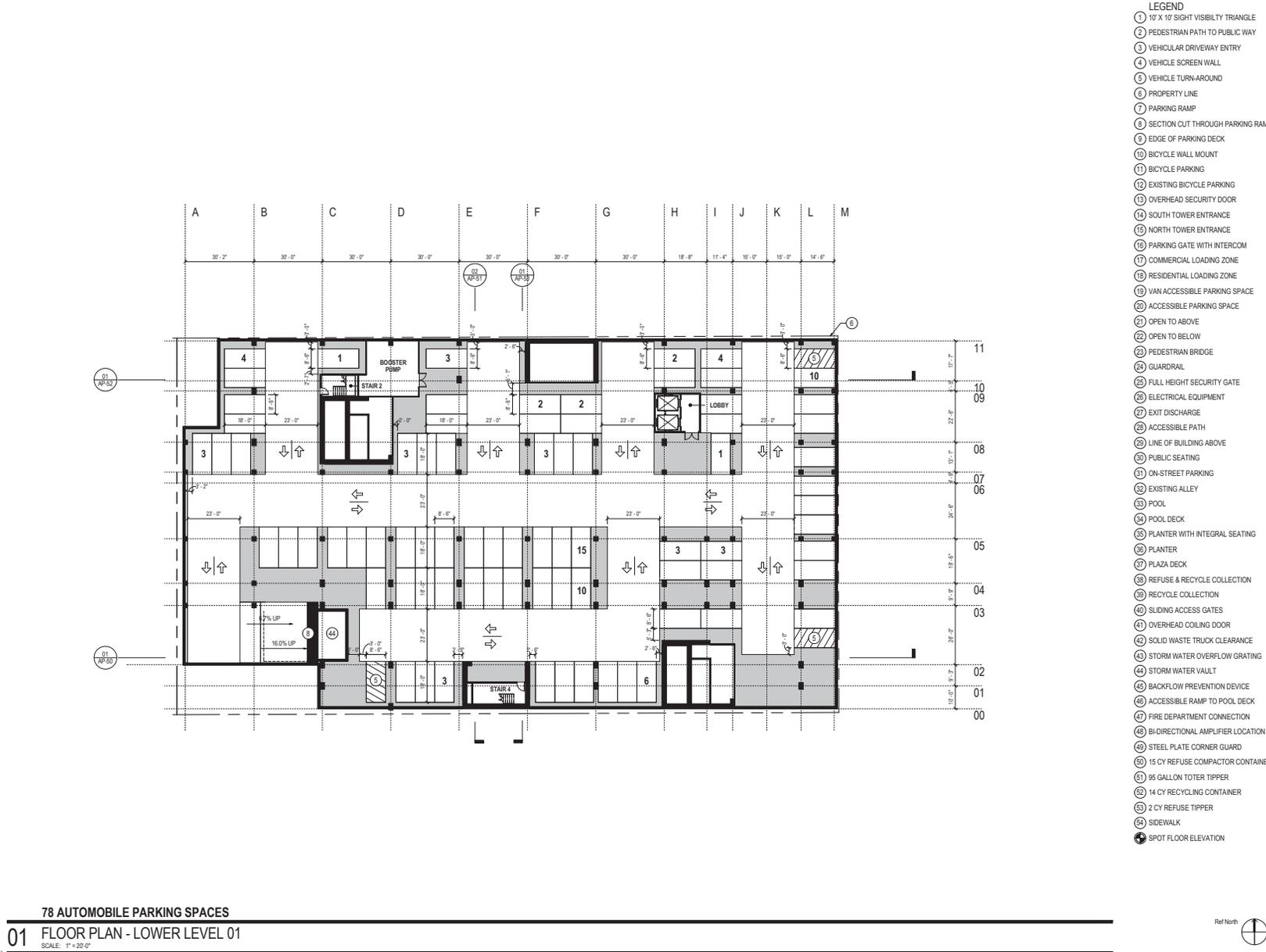
Description
BUILDING SECTIONS

Scale
As indicated

AP-52

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01 EAST WEST SECTION - 2
SCALE: 1/8" = 1'-0"



- LEGEND**
- ① 10' X 10' SIGHT VISIBILITY TRIANGLE
 - ② PEDESTRIAN PATH TO PUBLIC WAY
 - ③ VEHICULAR DRIVEWAY ENTRY
 - ④ VEHICLE SCREEN WALL
 - ⑤ VEHICLE TURN-AROUND
 - ⑥ PROPERTY LINE
 - ⑦ PARKING RAMP
 - ⑧ SECTION CUT THROUGH PARKING RAMP
 - ⑨ EDGE OF PARKING DECK
 - ⑩ BICYCLE WALL MOUNT
 - ⑪ BICYCLE PARKING
 - ⑫ EXISTING BICYCLE PARKING
 - ⑬ OVERHEAD SECURITY DOOR
 - ⑭ SOUTH TOWER ENTRANCE
 - ⑮ NORTH TOWER ENTRANCE
 - ⑯ PARKING GATE WITH INTERCOM
 - ⑰ COMMERCIAL LOADING ZONE
 - ⑱ RESIDENTIAL LOADING ZONE
 - ⑲ VAN ACCESSIBLE PARKING SPACE
 - ⑳ ACCESSIBLE PARKING SPACE
 - ㉑ OPEN TO ABOVE
 - ㉒ OPEN TO BELOW
 - ㉓ PEDESTRIAN BRIDGE
 - ㉔ GUARDRAIL
 - ㉕ FULL HEIGHT SECURITY GATE
 - ㉖ ELECTRICAL EQUIPMENT
 - ㉗ EXIT DISCHARGE
 - ㉘ ACCESSIBLE PATH
 - ㉙ LINE OF BUILDING ABOVE
 - ㉚ PUBLIC SEATING
 - ㉛ ON-STREET PARKING
 - ㉜ EXISTING ALLEY
 - ㉝ POOL
 - ㉞ POOL DECK
 - ㉟ PLANTER WITH INTEGRAL SEATING
 - ㊱ PLANTER
 - ㊲ PLAZA DECK
 - ㊳ REFUSE & RECYCLE COLLECTION
 - ㊴ RECYCLE COLLECTION
 - ㊵ SLIDING ACCESS GATES
 - ㊶ OVERHEAD COILING DOOR
 - ㊷ SOLID WASTE TRUCK CLEARANCE
 - ㊸ STORM WATER OVERFLOW GRATING
 - ㊹ STORM WATER VAULT
 - ㊺ BACKFLOW PREVENTION DEVICE
 - ㊻ ACCESSIBLE RAMP TO POOL DECK
 - ㊼ FIRE DEPARTMENT CONNECTION
 - ㊽ BI-DIRECTIONAL AMPLIFIER LOCATION
 - ㊾ STEEL PLATE CORNER GUARD
 - ㊿ 15 CY REFUSE COMPACTOR CONTAINER
 - 1 95 GALLON TOTER TIPPER
 - 2 14 CY RECYCLING CONTAINER
 - 3 2 CY REFUSE TIPPER
 - 4 SIDEWALK
 - 5 SPOT FLOOR ELEVATION

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Project Name
Newman Center / The Maxwell on College

Project Number
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Description
FLOOR PLAN - LOWER LEVEL

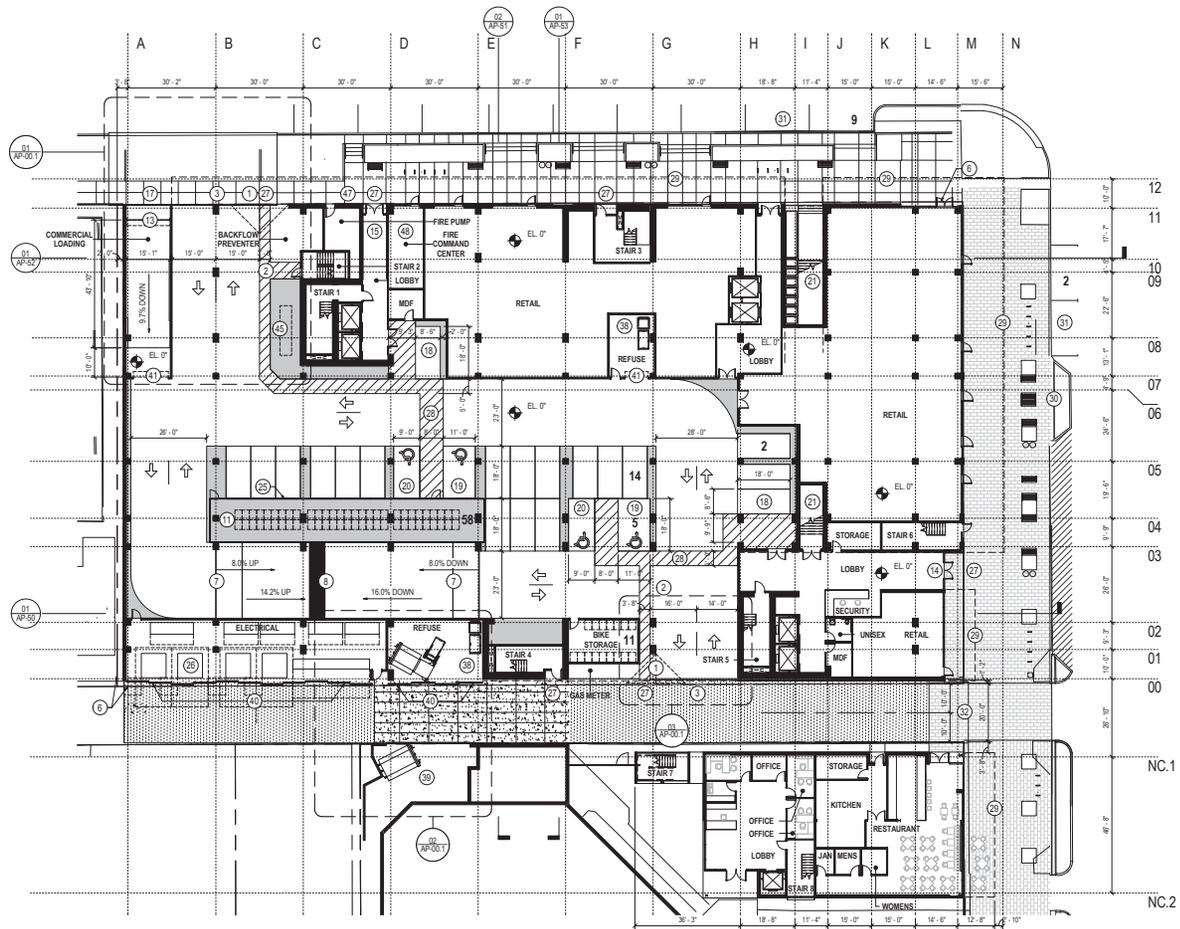
Scale
 As indicated

Ref North

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78 AUTOMOBILE PARKING SPACES
01 FLOOR PLAN - LOWER LEVEL 01
 SCALE: 1" = 20'-0"



21 AUTOMOBILE PARKING SPACES
01 FLOOR PLAN - LEVEL 01 - GROUND FLOOR
 SCALE: 1" = 20'-0"

- LEGEND**
- ① 10' X 10' SIGHT VISIBILITY TRIANGLE
 - ② PEDESTRIAN PATH TO PUBLIC WAY
 - ③ VEHICULAR DRIVEWAY ENTRY
 - ④ VEHICLE SCREEN WALL
 - ⑤ VEHICLE TURN-AROUND
 - ⑥ PROPERTY LINE
 - ⑦ PARKING RAMP
 - ⑧ SECTION CUT THROUGH PARKING RAMP
 - ⑨ EDGE OF PARKING DECK
 - ⑩ BICYCLE WALL MOUNT
 - ⑪ BICYCLE PARKING
 - ⑫ EXISTING BICYCLE PARKING
 - ⑬ OVERHEAD SECURITY DOOR
 - ⑭ SOUTH TOWER ENTRANCE
 - ⑮ NORTH TOWER ENTRANCE
 - ⑯ PARKING GATE WITH INTERCOM
 - ⑰ COMMERCIAL LOADING ZONE
 - ⑱ RESIDENTIAL LOADING ZONE
 - ⑲ VAN ACCESSIBLE PARKING SPACE
 - ⑳ ACCESSIBLE PARKING SPACE
 - ㉑ OPEN TO ABOVE
 - ㉒ OPEN TO BELOW
 - ㉓ PEDESTRIAN BRIDGE
 - ㉔ GUARDRAIL
 - ㉕ FULL HEIGHT SECURITY GATE
 - ㉖ ELECTRICAL EQUIPMENT
 - ㉗ EXIT DISCHARGE
 - ㉘ ACCESSIBLE PATH
 - ㉙ LINE OF BUILDING ABOVE
 - ㉚ PUBLIC SEATING
 - ㉛ ON-STREET PARKING
 - ㉜ EXISTING ALLEY
 - ㉝ POOL
 - ㉞ POOL DECK
 - ㉟ PLANTER WITH INTEGRAL SEATING
 - ㊱ PLANTER
 - ㊲ PLAZA DECK
 - ㊳ REFUSE & RECYCLE COLLECTION
 - ㊴ RECYCLE COLLECTION
 - ㊵ SLIDING ACCESS GATES
 - ㊶ OVERHEAD COILING DOOR
 - ㊷ SOLID WASTE TRUCK CLEARANCE
 - ㊸ STORM WATER OVERFLOW GRATING
 - ㊹ STORM WATER VAULT
 - ㊺ BACKFLOW PREVENTION DEVICE
 - ㊻ ACCESSIBLE RAMP TO POOL DECK
 - ㊼ FIRE DEPARTMENT CONNECTION
 - ㊽ BI-DIRECTIONAL AMPLIFIER LOCATION
 - ㊾ STEEL PLATE CORNER GUARD
 - ㊿ 15 CY REFUSE COMPACTOR CONTAINER
 - 1 95 GALLON TOTER TIPPER
 - 2 14 CY RECYCLING CONTAINER
 - 3 2 CY REFUSE TIPPER
 - 4 SIDEWALK
 - ⊙ SPOT FLOOR ELEVATION

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Project Name
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 Project Number
57.6119.000
 Description
FLOOR PLAN - GROUND FLOOR

Scale
 As indicated

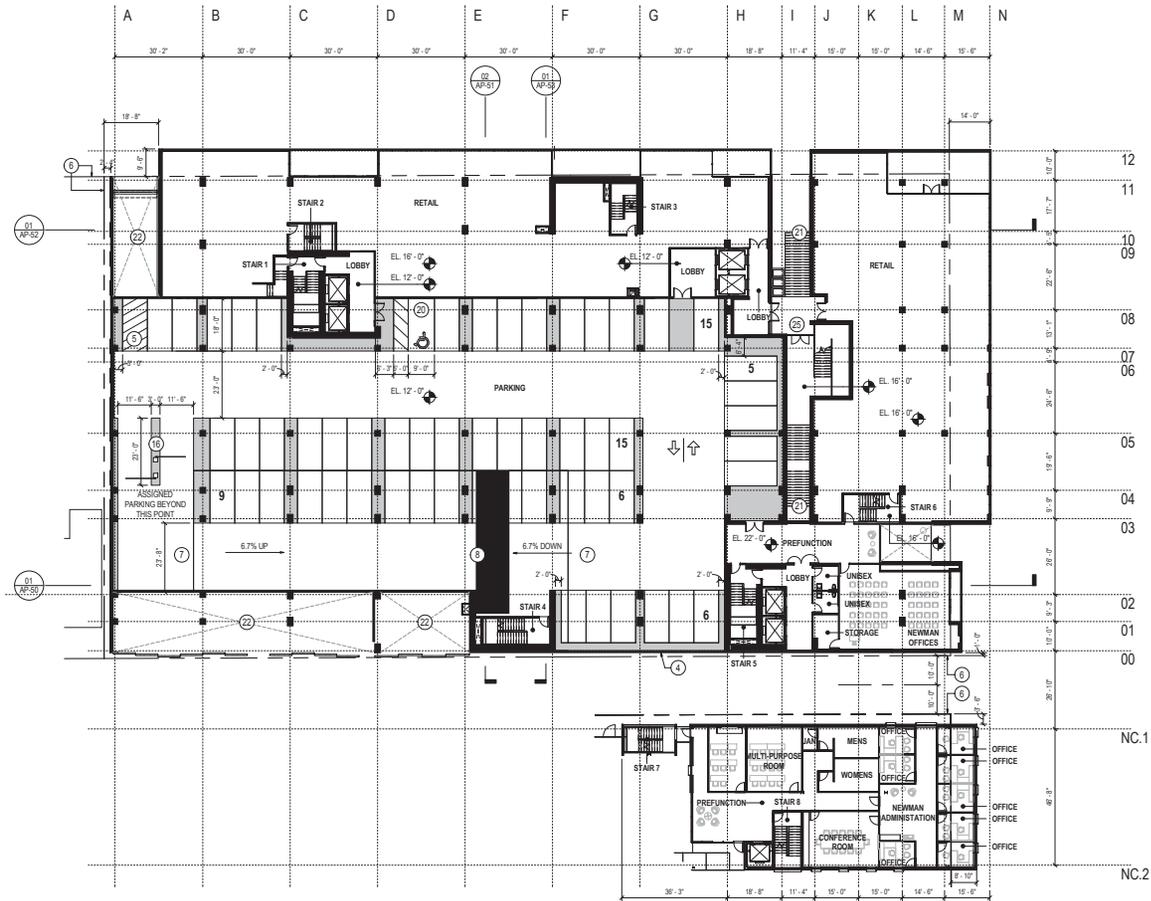
AP-01

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56 AUTOMOBILE PARKING SPACES

01 FLOOR PLAN - LEVEL 02

SCALE: 1" = 20'-0"



- LEGEND**
- ① 10' X 10' SIGHT VISIBILITY TRIANGLE
 - ② PEDESTRIAN PATH TO PUBLIC WAY
 - ③ VEHICULAR DRIVEWAY ENTRY
 - ④ VEHICLE SCREEN WALL
 - ⑤ VEHICLE TURN-AROUND
 - ⑥ PROPERTY LINE
 - ⑦ PARKING RAMP
 - ⑧ SECTION CUT THROUGH PARKING RAMP
 - ⑨ EDGE OF PARKING DECK
 - ⑩ BICYCLE WALL MOUNT
 - ⑪ BICYCLE PARKING
 - ⑫ EXISTING BICYCLE PARKING
 - ⑬ OVERHEAD SECURITY DOOR
 - ⑭ SOUTH TOWER ENTRANCE
 - ⑮ NORTH TOWER ENTRANCE
 - ⑯ PARKING GATE WITH INTERCOM
 - ⑰ COMMERCIAL LOADING ZONE
 - ⑱ RESIDENTIAL LOADING ZONE
 - ⑲ VAN ACCESSIBLE PARKING SPACE
 - ⑳ ACCESSIBLE PARKING SPACE
 - ㉑ OPEN TO ABOVE
 - ㉒ OPEN TO BELOW
 - ㉓ PEDESTRIAN BRIDGE
 - ㉔ GUARDRAIL
 - ㉕ FULL HEIGHT SECURITY GATE
 - ㉖ ELECTRICAL EQUIPMENT
 - ㉗ EXIT DISCHARGE
 - ㉘ ACCESSIBLE PATH
 - ㉙ LINE OF BUILDING ABOVE
 - ㉚ PUBLIC SEATING
 - ㉛ ON-STREET PARKING
 - ㉜ EXISTING ALLEY
 - ㉝ POOL
 - ㉞ POOL DECK
 - ㉟ PLANTER WITH INTEGRAL SEATING
 - ㊱ PLANTER
 - ㊲ PLAZA DECK
 - ㊳ REFUSE & RECYCLE COLLECTION
 - ㊴ RECYCLE COLLECTION
 - ㊵ SLIDING ACCESS GATES
 - ㊶ OVERHEAD COILING DOOR
 - ㊷ SOLID WASTE TRUCK CLEARANCE
 - ㊸ STORM WATER OVERFLOW GRATING
 - ㊹ STORM WATER VAULT
 - ㊺ BACKFLOW PREVENTION DEVICE
 - ㊻ ACCESSIBLE RAMP TO POOL DECK
 - ㊼ FIRE DEPARTMENT CONNECTION
 - ㊽ BI-DIRECTIONAL AMPLIFIER LOCATION
 - ㊾ STEEL PLATE CORNER GUARD
 - ㊿ 15 CY REFUSE COMPACTOR CONTAINER
 - ① 95 GALLON TOTER TIPPER
 - ② 14 CY RECYCLING CONTAINER
 - ③ 2 CY REFUSE TIPPER
 - ④ SIDEWALK
 - ⊙ SPOT FLOOR ELEVATION

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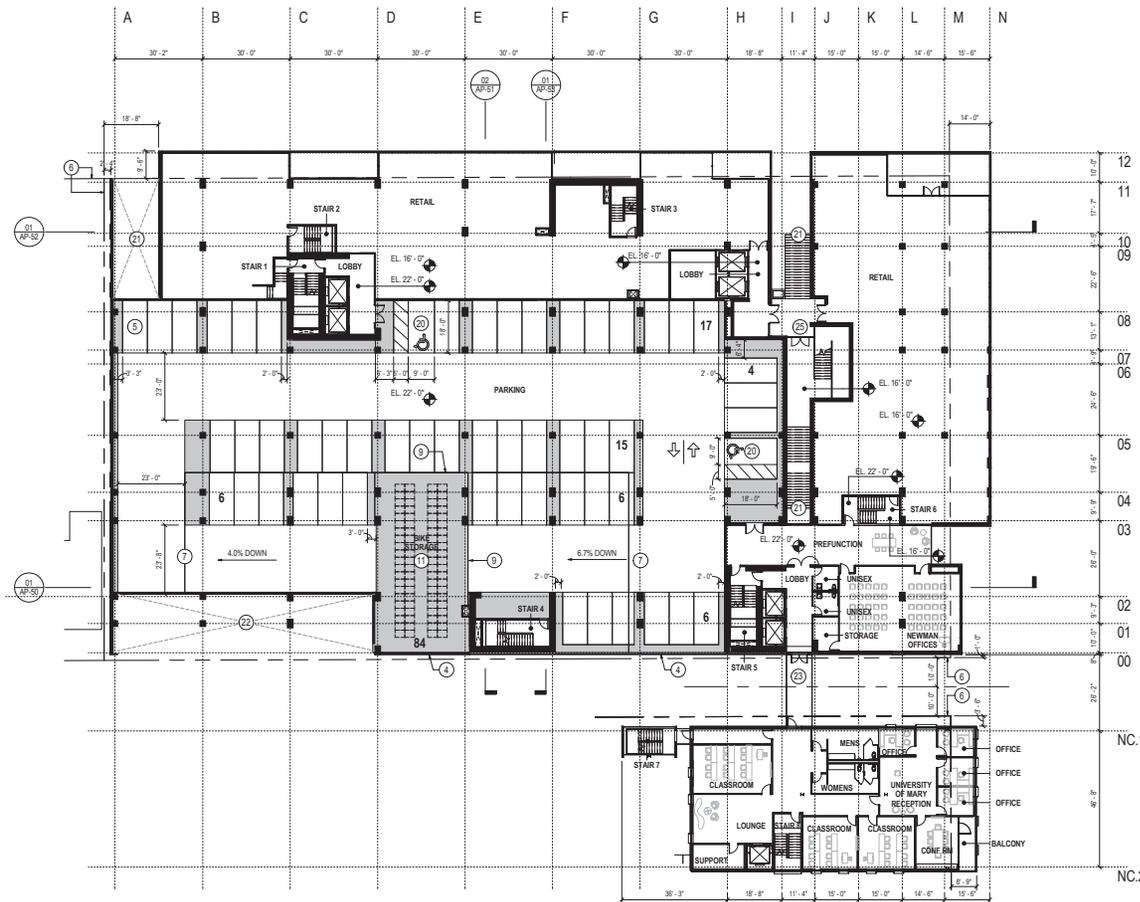
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Description
FLOOR PLAN - LEVEL 02

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53 AUTOMOBILE PARKING SPACES
01 FLOOR PLAN - LEVEL 03
 SCALE: 1" = 20'-0"

- LEGEND**
- ① 10' X 10' SIGHT VISIBILITY TRIANGLE
 - ② PEDESTRIAN PATH TO PUBLIC WAY
 - ③ VEHICULAR DRIVEWAY ENTRY
 - ④ VEHICLE SCREEN WALL
 - ⑤ VEHICLE TURN-AROUND
 - ⑥ PROPERTY LINE
 - ⑦ PARKING RAMP
 - ⑧ SECTION CUT THROUGH PARKING RAMP
 - ⑨ EDGE OF PARKING DECK
 - ⑩ BICYCLE WALL MOUNT
 - ⑪ BICYCLE PARKING
 - ⑫ EXISTING BICYCLE PARKING
 - ⑬ OVERHEAD SECURITY DOOR
 - ⑭ SOUTH TOWER ENTRANCE
 - ⑮ NORTH TOWER ENTRANCE
 - ⑯ PARKING GATE WITH INTERCOM
 - ⑰ COMMERCIAL LOADING ZONE
 - ⑱ RESIDENTIAL LOADING ZONE
 - ⑲ VAN ACCESSIBLE PARKING SPACE
 - ⑳ ACCESSIBLE PARKING SPACE
 - ㉑ OPEN TO ABOVE
 - ㉒ OPEN TO BELOW
 - ㉓ PEDESTRIAN BRIDGE
 - ㉔ GUARDRAIL
 - ㉕ FULL HEIGHT SECURITY GATE
 - ㉖ ELECTRICAL EQUIPMENT
 - ㉗ EXIT DISCHARGE
 - ㉘ ACCESSIBLE PATH
 - ㉙ LINE OF BUILDING ABOVE
 - ㉚ PUBLIC SEATING
 - ㉛ ON-STREET PARKING
 - ㉜ EXISTING ALLEY
 - ㉝ POOL
 - ㉞ POOL DECK
 - ㉟ PLANTER WITH INTEGRAL SEATING
 - ㊱ PLANTER
 - ㊲ PLAZA DECK
 - ㊳ REFUSE & RECYCLE COLLECTION
 - ㊴ RECYCLE COLLECTION
 - ㊵ SLIDING ACCESS GATES
 - ㊶ OVERHEAD COILING DOOR
 - ㊷ SOLID WASTE TRUCK CLEARANCE
 - ㊸ STORM WATER OVERFLOW GRATING
 - ㊹ STORM WATER VAULT
 - ㊺ BACKFLOW PREVENTION DEVICE
 - ㊻ ACCESSIBLE RAMP TO POOL DECK
 - ㊼ FIRE DEPARTMENT CONNECTION
 - ㊽ BI-DIRECTIONAL AMPLIFIER LOCATION
 - ㊾ STEEL PLATE CORNER GUARD
 - ㊿ 15 CY REFUSE COMPACTOR CONTAINER
 - ① 95 GALLON TOTER TIPPER
 - ② 14 CY RECYCLING CONTAINER
 - ③ 2 CY REFUSE TIPPER
 - ④ SIDEWALK
 - ⊙ SPOT FLOOR ELEVATION

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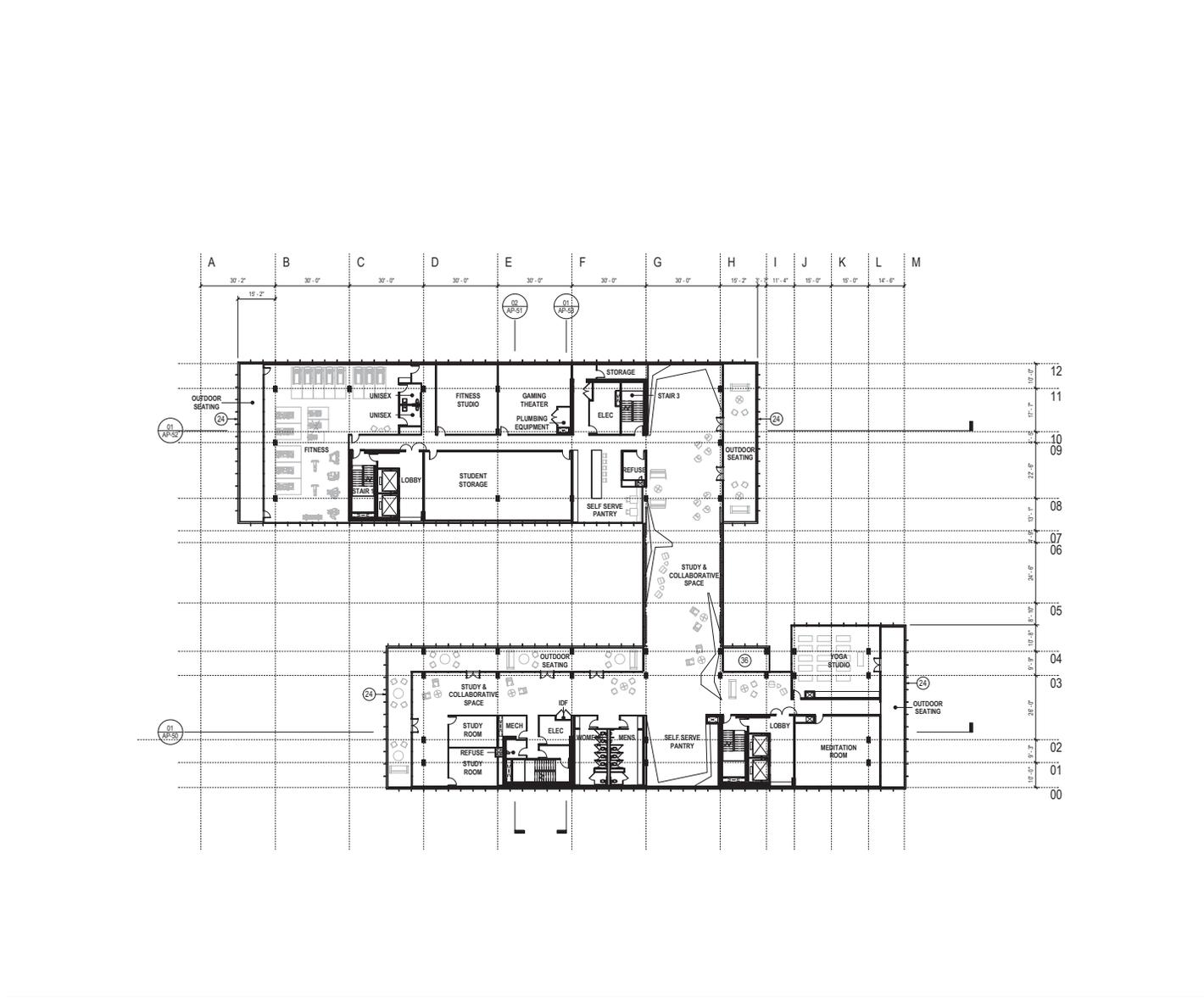
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Description
FLOOR PLAN - LEVEL 03

Scale
 As indicated

AP-03

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- LEGEND**
- ① 10' X 10' SIGHT VISIBILITY TRIANGLE
 - ② PEDESTRIAN PATH TO PUBLIC WAY
 - ③ VEHICULAR DRIVEWAY ENTRY
 - ④ VEHICLE SCREEN WALL
 - ⑤ VEHICLE TURN-AROUND
 - ⑥ PROPERTY LINE
 - ⑦ PARKING RAMP
 - ⑧ SECTION CUT THROUGH PARKING RAMP
 - ⑨ EDGE OF PARKING DECK
 - ⑩ BICYCLE WALL MOUNT
 - ⑪ BICYCLE PARKING
 - ⑫ EXISTING BICYCLE PARKING
 - ⑬ OVERHEAD SECURITY DOOR
 - ⑭ SOUTH TOWER ENTRANCE
 - ⑮ NORTH TOWER ENTRANCE
 - ⑯ PARKING GATE WITH INTERCOM
 - ⑰ COMMERCIAL LOADING ZONE
 - ⑱ RESIDENTIAL LOADING ZONE
 - ⑲ VAN ACCESSIBLE PARKING SPACE
 - ⑳ ACCESSIBLE PARKING SPACE
 - ㉑ OPEN TO ABOVE
 - ㉒ OPEN TO BELOW
 - ㉓ PEDESTRIAN BRIDGE
 - ㉔ GUARDRAIL
 - ㉕ FULL HEIGHT SECURITY GATE
 - ㉖ ELECTRICAL EQUIPMENT
 - ㉗ EXIT DISCHARGE
 - ㉘ ACCESSIBLE PATH
 - ㉙ LINE OF BUILDING ABOVE
 - ㉚ PUBLIC SEATING
 - ㉛ ON-STREET PARKING
 - ㉜ EXISTING ALLEY
 - ㉝ POOL
 - ㉞ POOL DECK
 - ㉟ PLANTER WITH INTEGRAL SEATING
 - ㊱ PLANTER
 - ㊲ PLAZA DECK
 - ㊳ REFUSE & RECYCLE COLLECTION
 - ㊴ RECYCLE COLLECTION
 - ㊵ SLIDING ACCESS GATES
 - ㊶ OVERHEAD COILING DOOR
 - ㊷ SOLID WASTE TRUCK CLEARANCE
 - ㊸ STORM WATER OVERFLOW GRATING
 - ㊹ STORM WATER VAULT
 - ㊺ BACKFLOW PREVENTION DEVICE
 - ㊻ ACCESSIBLE RAMP TO POOL DECK
 - ㊼ FIRE DEPARTMENT CONNECTION
 - ㊽ BI-DIRECTIONAL AMPLIFIER LOCATION
 - ㊾ STEEL PLATE CORNER GUARD
 - ㊿ 15 CY REFUSE COMPACTOR CONTAINER
 - 1 95 GALLON TOTER TIPPER
 - 2 14 CY RECYCLING CONTAINER
 - 3 2 CY REFUSE TIPPER
 - 4 SIDEWALK
 - 5 SPOT FLOOR ELEVATION

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Date	Description
02.22.16	Amended Site Plan Submittal
03.14.16	Amended Site Plan Submittal

Seal/Signature
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Project Name
**Newnam Center / The
Maxwell on College**

Project Number
57.6119.000

Description
**FLOOR PLAN - LEVEL 08 - BRIDGE
LEVEL**

Scale
As indicated

AP-06

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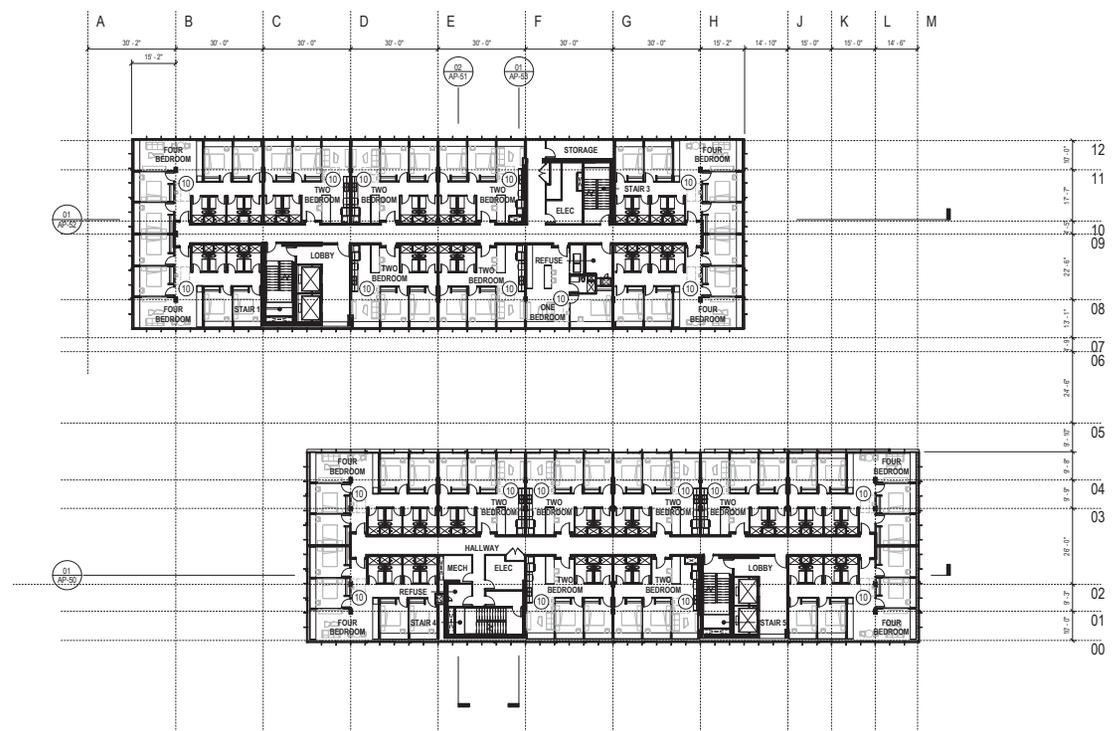
01 FLOOR PLAN - LEVEL 08 - BRIDGE LEVEL
SCALE: 1" = 20'-0"

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- LEGEND
- ① 10' X 10' SIGHT VISIBILITY TRIANGLE
 - ② PEDESTRIAN PATH TO PUBLIC WAY
 - ③ VEHICULAR DRIVEWAY ENTRY
 - ④ VEHICLE SCREEN WALL
 - ⑤ VEHICLE TURN-AROUND
 - ⑥ PROPERTY LINE
 - ⑦ PARKING RAMP
 - ⑧ SECTION CUT THROUGH PARKING RAMP
 - ⑨ EDGE OF PARKING DECK
 - ⑩ BICYCLE WALL MOUNT
 - ⑪ BICYCLE PARKING
 - ⑫ EXISTING BICYCLE PARKING
 - ⑬ OVERHEAD SECURITY DOOR
 - ⑭ SOUTH TOWER ENTRANCE
 - ⑮ NORTH TOWER ENTRANCE
 - ⑯ PARKING GATE WITH INTERCOM
 - ⑰ COMMERCIAL LOADING ZONE
 - ⑱ RESIDENTIAL LOADING ZONE
 - ⑲ VAN ACCESSIBLE PARKING SPACE
 - ⑳ ACCESSIBLE PARKING SPACE
 - ㉑ OPEN TO ABOVE
 - ㉒ OPEN TO BELOW
 - ㉓ PEDESTRIAN BRIDGE
 - ㉔ GUARDRAIL
 - ㉕ FULL HEIGHT SECURITY GATE
 - ㉖ ELECTRICAL EQUIPMENT
 - ㉗ EXIT DISCHARGE
 - ㉘ ACCESSIBLE PATH
 - ㉙ LINE OF BUILDING ABOVE
 - ㉚ PUBLIC SEATING
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 - ⑤ SPOT FLOOR ELEVATION



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03.14.16	Amended Site Plan Submittal

Seal/Signature
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Project Name
Newman Center / The Maxwell on College
 Project Number
57.6119.000

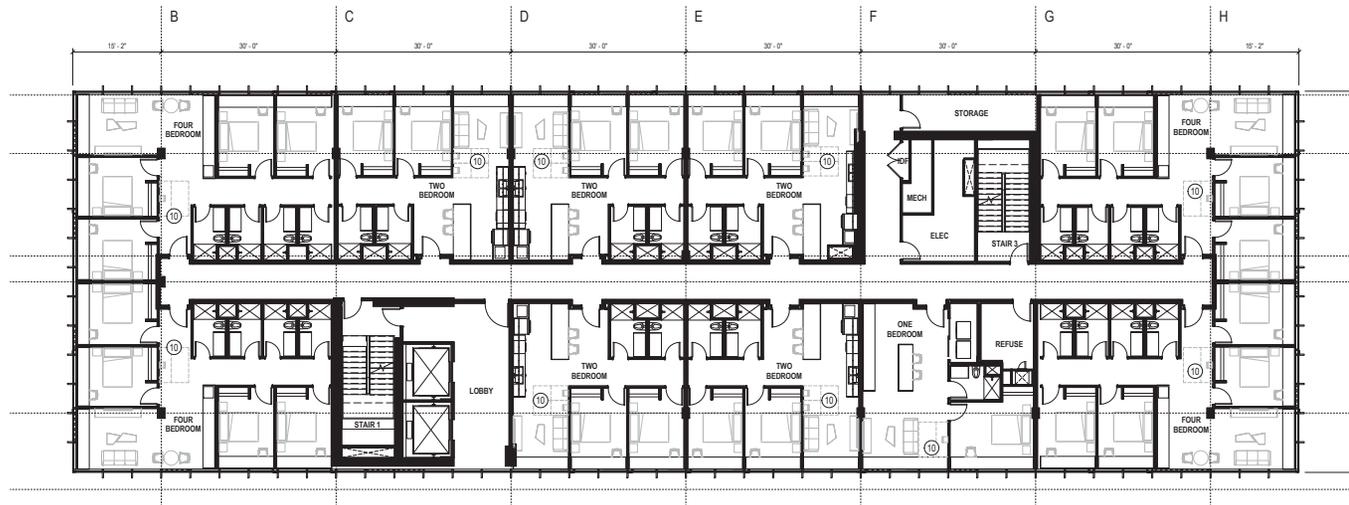
Description
FLOOR PLAN - TYPICAL LEVEL 9-20

Scale
 As indicated

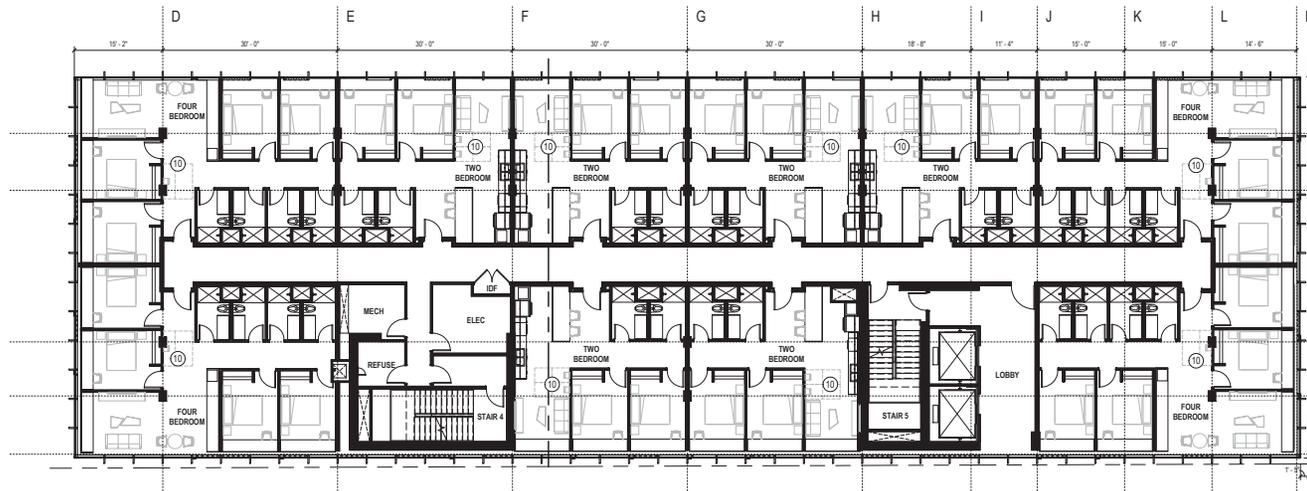
AP-07

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01 FLOOR PLAN-TYP LEVEL 9-18 NORTH TOWER, LEVEL 9-20 SOUTH TOWER
 SCALE: 1" = 20'-0"



01 FLOOR PLAN - NORTH TOWER - TYPICAL ENLARGED FLOOR PLANS
SCALE: 1" = 10'-0"



02 FLOOR PLAN - SOUTH TOWER - TYPICAL ENLARGED FLOOR PLANS
SCALE: 1" = 10'-0"

- LEGEND**
- ① 10' X 10' SIGHT VISIBILITY TRIANGLE
 - ② PEDESTRIAN PATH TO PUBLIC WAY
 - ③ VEHICULAR DRIVEWAY ENTRY
 - ④ VEHICLE SCREEN WALL
 - ⑤ VEHICLE TURN-AROUND
 - ⑥ PROPERTY LINE
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Project Name
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Project Number
57.6119.000

Description
FLOOR PLAN - ENLARGED FLOOR PLANS

Scale
As indicated

AP-08

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View from University Drive and College Avenue - Looking North



View from 7th Street and College Avenue - Looking South

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Project Number

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Description

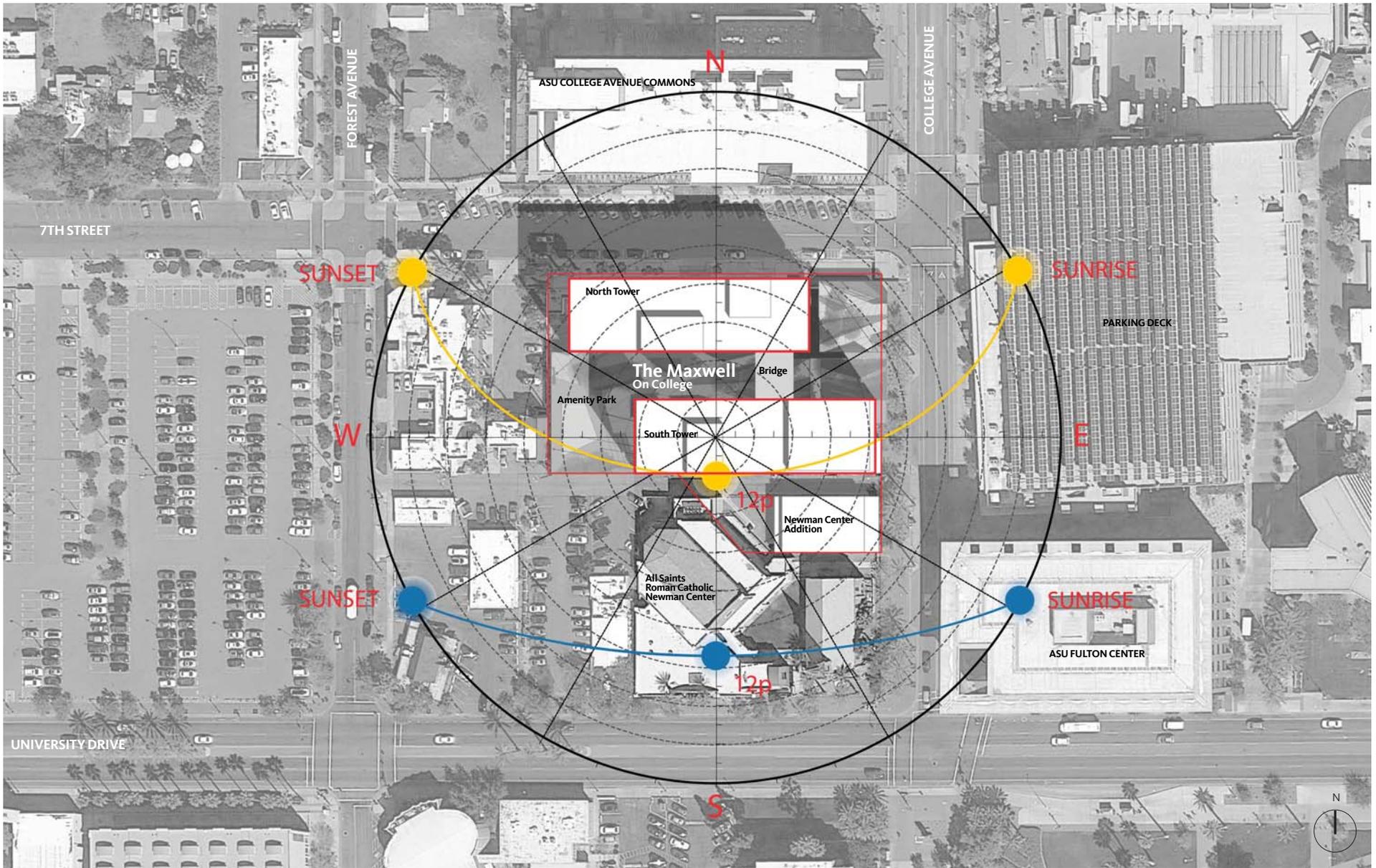
PERSPECTIVE VIEWS

Scale

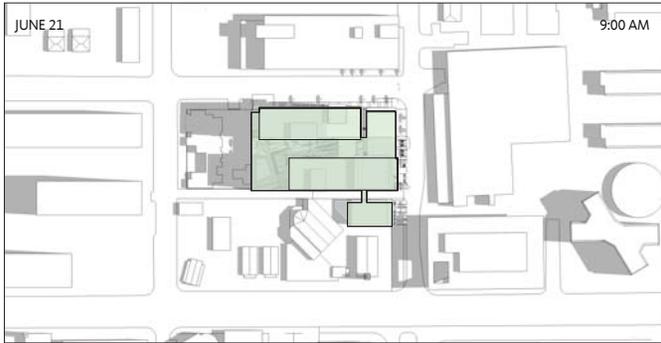
1/2" = 1'-0"

AP-44

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UNIVERSITY of MARY | for Life.







NATIVE SHADE TREES



CIP CONCRETE PAVING



WOOD DECKING



SCULPTURAL PLANTING



TERRACED PLANTING



TERRACED SEATING



TERRACED DECKS



SCULPTURAL PLANTING



ANGULAR LANDFORMS



ANGULAR WALL AND INTIMATE SEATING



INTIMATE SEATING



OUTDOOR SCREEN

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Date	Description
02.22.16	Amended Site Plan Submittal
03.14.16	Amended Site Plan Submittal



Project Name

Newman Center / The Maxwell on College

Project Number

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Description

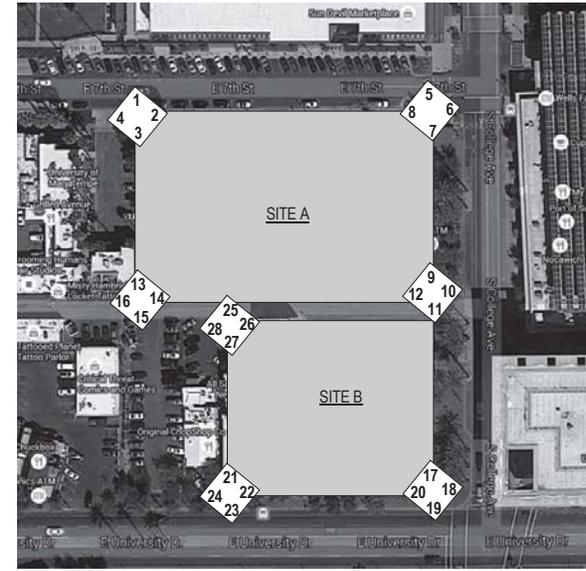
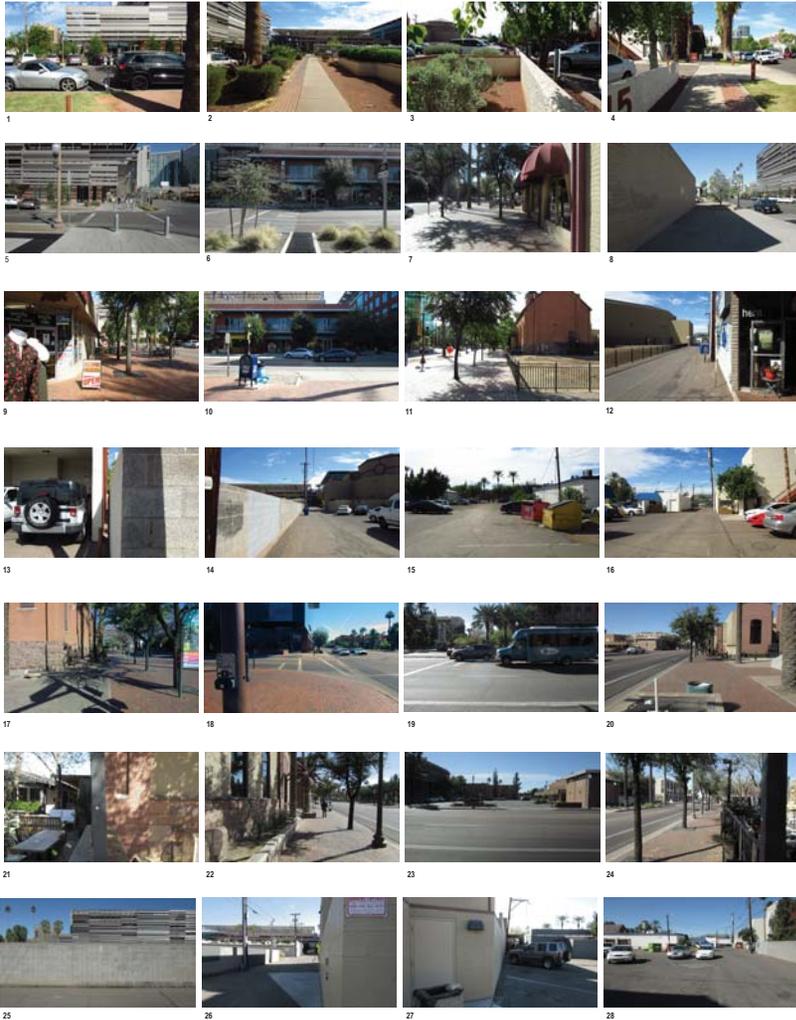
LANDSCAPE CHARACTER LEVEL 4

Scale

N/A

L1.03

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1 SITE PHOTO DIAGRAM
SCALE: NTS

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Project Name

Newman Center / The
Maxwell on College

Project Number

57.6119.000

Description

SITE PHOTOS

Scale

As indicated



AP-60

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March 14, 2016

John Williams
Gensler
201 East Washington Street, Suite 750
Phoenix, Arizona 85004



Expires 12-31-2018

RE: Parking Study for Newman Center/The Maxwell on College- Tempe, Arizona

Dear Mr. Williams:

CivTech has completed a parking study for the proposed Newman Center/The Maxwell on College mixed-use development. The project is proposed in the southwest corner of College Avenue and 7th Street in the City of Tempe, Arizona. A detailed parking analysis has been requested by the City of Tempe to fulfill the application for development. This parking study documents CivTech's findings and clarifies any disparity between the number of spaces required and the number of spaces provided. The parking study has been completed in accordance with the City of Tempe standards as presented in their Zoning and Development Code.

PROPOSED DEVELOPMENT

The site currently consists of a parking lot and a retail building, which will be redeveloped into 'Newman Center/The Maxwell on College'. The location of the site is in downtown Tempe and adjacent to the ASU campus. Access to the development will be via 7th Street and an alley to the south.

The proposed development will provide a total of 217 parking spaces. The total parking spaces is comprised of 10 on-street parking spaces for commercial and office, 127 parking spaces in the garage for commercial and guests, and 80 parking spaces reserved for residents. Bicycle spaces/storage will be provided on-site at a ratio of one per bedroom. Surface bike racks will also be provided on-street for the commercial uses. At full build-out, the project will consist of 295 apartment units (798 bedrooms), retail, restaurant, and office space. The land uses are summarized in **Table 1**, and the mixture of apartment types is summarized in **Table 2**.

Table 1: Proposed Land Uses

Land Use	Size
SITE A:	
Residential	294 units (795 bedrooms)
Retail	12,870 SF
Office	14,960 SF
Restaurant	14,520 SF
SITE B:	
Residential	1 unit (3 bedrooms)
Office	9,153 SF
Restaurant	2,703 SF
Classroom	3,249 SF
Existing Office	3,345 SF
Existing Sanctuary/Church	18,306 SF

Table 2: Proposed Residential Units

Apartment Type	Number of Units		Number of Bedrooms	
	Site A	Site B	Site A	Site B
Studio	0	0	0	0
One-Bedroom	23	0	23	0
Two-Bedroom	156	0	312	0
Three-Bedroom	0	1	0	3
Four-Bedroom	115	0	460	0
TOTAL	294	1	795	3

SITE'S PROXIMITY TO ALTERNATE MODES OF TRANSPORTATION

Per the Tempe General Plan 2040, "Tempe's vision for itself in the year 2040 is one of livability: a city with a diverse, active and engaged community; a city that is visually attractive and accessible by multiple modes of transportation." And consisting of "revitalized neighborhoods that are walkable, pleasant and safe, and connected within a 20-minute walk, bike or transit ride." Tempe's goal of connecting neighborhoods within 20-minutes via multiple modes of transportation has resulted in numerous transit options in close proximity to the proposed site.

LIGHT RAIL

The project site is approximately 1,200 feet from the center of the light rail station platform on Veteran's Way at the Tempe Transportation Center.

BUS ROUTES

The site is located within a ¼ mile of the Tempe Transportation Center on Veteran's Way, which is a major multi-modal transportation hub. Dozens of bus routes serve the center including the free Orbit local shuttle.

College Avenue provides bus stops for bus routes 62 and 72 and Tempe Orbits: Mercury, Mars, and Venus. Within the vicinity of the site, University Drive provides bus stops for bus routes 48, 62, 65, 66, ASU FLASH, Tempe Orbits: Earth, Mars, Mercury, Jupiter and Venus.

PEDESTRIAN/BICYCLE

Tempe has a long-standing commitment to encourage bicycling since the inception of the Tempe Bicycle Program. The proposed development will have a high volume of pedestrian and bicycle activity due to its close proximity to ASU. Bicycle spaces/storage will be provided on-site at a ratio of one per bedroom. The proposed sidewalks along the 7th Street and College Avenue frontages tie into the existing pedestrian facilities and are pedestrian friendly with ample landscaping and benches. The development is being designed with pedestrians and bicyclists in mind and will provide sidewalks that preserve the character of downtown Tempe.

College Avenue and University Drive both have dedicated bicycle lanes in both directions of traffic. The proposed site is located just outside of the ASU campus making pedestrian and bicycle activity convenient for accessing the ASU campus. The farthest side of the ASU campus may be reached within 15 minutes by walking, and the majority of campus is reached within 5-10 minutes. The Tempe Transportation Center can be reached within 5 minutes by walking, and the majority of downtown Tempe on Mill Avenue can be reached within 10 minutes by walking. The area around the ASU campus is designed to be pedestrian friendly.

TEMPE'S TRANSPORTATION OVERLAY DISTRICT (TOD)

The site is within the City of Tempe's Transportation Overlay District (TOD). The project site is approximately 1,200 feet from the center of the light rail station platform on Veteran's Way at the Tempe Transportation Center.

CITY OF TEMPE PARKING REQUIREMENTS

Ordinance no. 02015.60 – City Center (CC) District

The City of Tempe recently approved an ordinance amending the Zoning and Development Code, Part 4, Chapter 6 – Parking, Section 4-603 and adding a new section 4-607 – Downtown Parking Standards. This section provides new off-street parking standards for developments within the City Center District. The site of the proposed Maxwell on College development is within the City Center (CC) district. **Table 3** summarizes the required parking per the ratios in Table 4-607A (CC District Parking Standards).

**Table 3: Required Parking Spaces per City of Tempe’s City Center (CC) District
 Parking Standards**

Residential				Required Spaces per ZDC	
Apartment Type	Number of Units	Number of Bedrooms	Required Ratios per City Center District ⁽¹⁾	Vehicle	Bicycle ⁽²⁾
One-Bedroom	23	23	0.50 space per bdrm 0.75 bicycle per unit	11.50	17.25
Two-Bedroom	156	312	0.50 space per bdrm 0.75 bicycle per unit	156.00	117.00
Three-Bedroom	1	3	0.30 space per bdrm 1.0 bicycle per unit	0.90	1.00
Four-Bedroom	115	460	0.30 space per bdrm 1.0 bicycle per unit	138.00	115.00
Guest Parking	295	798	0.1 space per unit 0.2 bicycle per unit	29.50	59.00
Total Residential Parking				335.90	309.25
Non-Residential				Required Spaces per ZDC	
Use	Size	Required Ratios per City Center District ⁽¹⁾		Vehicle	Bicycle ⁽²⁾
Commercial	54,206 SF	1.0 space per 500 SF ⁽⁴⁾		98.41	
Retail	12,870 SF	1.0 bicycle/7,500 SF			1.72
Office	24,113 SF	1.0 bicycle/8,000 SF ⁽³⁾			34.45
Restaurant	17,223 SF	1.0 bicycle/500 SF			3.01
Classroom	3,249 SF	1.0 space per 350 SF ⁽⁴⁾ 1.0 bicycle/1,500 SF		9.28	2.17
Total Non-Residential Parking				107.69	41.34
Total Required Parking				443.59	350.59
Existing Newman Center ⁽⁵⁾				Required Spaces per ZDC	
Use	Size	Required Ratios per City Center District ⁽¹⁾		Vehicle	Bicycle ⁽²⁾
Existing Sanctuary	18,306 SF	0.0 space per 300 SF ⁽⁵⁾ 0.0 bicycle/1,500 SF		0	0
Existing Office	3,345 SF	0.0 space per 500 SF ⁽⁵⁾ 0.0 bicycle/8,000 SF		0	0
Total Required Parking for Existing Newman Center				0	0
GRAND TOTAL				443.59	350.59

1. The parking and bicycle ratios are from proposed Table 4-607A for the City Center (CC) District.
2. The bicycle parking ratios are based on those required for the ‘Bicycle Commute Area.’
3. Bicycle parking - 4 spaces minimum. (The minimum is accounted for in the overall total of the mixed uses.)
4. Per proposed Table 4-607A, the required parking for the total commercial waives the first 5,000 SF and then 1 space per 500 SF thereafter.
5. The existing Newman Center office is being converted to storage, and the Newman Center has an agreement with the ASU Foundation to utilize their garage for parking.

As summarized in **Table 3**, the required parking based on the proposed City Center (CC) requirements is 336 parking spaces for the residential and 108 parking spaces for the non-residential uses. The existing Newman Center office is being converted to storage and the Newman Center has an agreement with the ASU Foundation to utilize their garage for parking.

The proposed development will provide a total of 217 parking spaces. The total parking spaces is comprised of 10 on-street parking spaces for commercial and office, 127 parking spaces in the garage for commercial and guests, and 80 parking spaces reserved for residents. Therefore, 137 parking spaces are provided for guests and commercial uses. The provided parking for the residential use results in a deficit per the City Center (CC) requirements.

The project in total will be providing 859 bicycle parking spaces. Of these bicycle spaces, 61 will be provided on-street for commercial uses and guests.

The resident parking will be limited to 80 rented permits. The garage parking spaces designated for commercial and office uses will be controlled by the use of metering to discourage student residents from utilizing them. Most of the student residents will be restricted to bicycle spaces. Due to the close proximity of the Tempe Transportation center (less than ¼ mile to the north), ASU Campus (less than ¼ mile to the south), and Mill Avenue, student residents will have a reduced need for personal vehicles. The student housing will be marketed with the limited parking.

PARKING MANAGEMENT PLAN

The proposed development will provide a total of 217 parking spaces. The total parking spaces is comprised of 10 on-street parking spaces for commercial uses, 127 parking spaces in the garage for commercial and guests, and 80 parking spaces reserved for residents. The number of parking spaces is summarized in **Table 4**. As required by City Staff, none of the bicycle parking will be provided on the lower level, and the project in total will be providing 859 bicycle parking spaces, more than twice required by code. Of these bicycle spaces, 61 will be provided on-street for commercial uses and guests.

Table 4: Provided Vehicle Parking

	Number
Commercial and guest parking in garage	127
Resident parking in garage	80
<i>Total Garage</i>	<i>207</i>
<i>Commercial and guest parking on-street</i>	<i>10</i>
TOTAL	217

The required parking based on the proposed City Center (CC) requirements is 336 parking spaces for the residential use, and 108 parking spaces for the non-residential uses.

Within the garage, 127 parking spaces will be designated for guest parking, commercial uses and the Newman Center offices and 10 spaces will be provided on-street for a total of 137 parking spaces for commercial and guests. The residents will have 80 reserved parking spaces in the garage separated by a gate. The garage parking spaces designated for commercial uses will be controlled by the use of metering to discourage student residents from utilizing them.

The existing Newman Center has an agreement with the ASU Foundation to utilize their parking garage during the weekend for church activities. Because the student residents will have a reduced need for personal vehicles, limited parking has been provided. Should the land use change in the future such as to a hotel, the lower level of the garage has a height of 15 feet

allowing for automated parking to be installed if needed. Off-site parking would also be procured at that time if necessary.

While the project is trying to discourage vehicular ownership and use in this very urban area of Tempe by the students who will be living here, they do acknowledge the need to provide parking for all of the commercial uses, all of the residential guests, and some of the residential units that are likely to house couples, which may include one member not attending ASU or working in the downtown area. Vehicle parking on-site will be provided on-street and in the on-site garage accessed from 7th Street and the alley. The on-street vehicular parking will include nine (9) parallel stalls on 7th Street and one (1) parallel stall on College Avenue, at the corner with 7th Street. These will continue to be metered by the City's system and will provide fifteen percent (15%) of the parking for the commercial uses north of the alley.

By interviewing other urban student housing projects, the project has learned that parking in these structures is often very expensive long-term car storage. While students are charged monthly for the right to park in the associated structures, they often find more affordable locations to store vehicles they rarely use because of the surrounding urban context. This often leads to expensive under-utilized parking structures. In an attempt to find the appropriate balance for urban student housing, the upper most levels of the parking garage will be gated and reserved with eighty (80) residential spaces. Per the CC District Parking Code, twenty-nine percent (29%) of the code required vehicular parking for residential units (85 of 294 units) will be fully accommodated by these stalls. These will provide the code required vehicular parking for all of the one (1) bedroom units and the code required vehicular parking for forty percent (40%) of two (2) bedroom units. Vehicular stalls are provided for these units as they are the unit types most likely to have a partner living in the unit who may not be an ASU student or work outside of the downtown area. To offset the rest of the code required vehicular parking for these unit types (1 & 2 bedroom), the project will be providing more than two and a half (2 ½) times the code required bicycle parking for these units. The only three (3) bedroom unit is the rectory and as a live/work unit is covered by the parking reserved for the Newman Center described below. For the convenience of the rectory residents, the project will also be providing three (3) times the required bicycle parking for this unit. The four (4) bedroom units are the most dorm like and most likely to be all students because of the unit type (shared baths, limited shared common space, etc.) and being leased by the bed. Instead of providing vehicular parking for these units, the project will be providing four (4) times the required bicycle parking.

The ground level, lower level and most of the second level parking will be semi-public paid/metered or reserved parking for the commercial uses, the required residential guest parking, and for the Newman Center and University of Mary's daily use (28 stalls). The number of vehicular parking stalls required for the Newman Center and University of Mary's daily use was determined through extensive interviews with Michele Kilker, Business Manager for the Newman Center. Per their vast experience managing the existing private surface parking lot north of the alley, they are vitally aware of how many spaces are needed for the Newman Center staff and daily visitors. The Newman facilities rarely (if ever) have all of the staff and additional visitors on-site at the same time. For the past six (6) years, Newman has leased out most of the existing parking spaces on an hourly, daily or semester long basis reserving five (5) to eight (8) for staff uses and keeping three (3) to five (5) available for guests/visitors. Estimating the long term growth of the activities at the center, and incorporating the University of Mary into the site (currently in an adjacent property), Newman anticipated that its needs will grow to twenty (20) daily spaces with eight (8) additional spaces for the University of Mary. In total, these twenty-eight (28) spaces will provide all of the required vehicle parking spaces for the commercial uses and eighty-five percent (85%) of the classroom uses south of the alley except the existing office space that will be converted into storage space (in the basement of

Historic Old Saint Mary's) and the restaurant space that is separately accommodated in the garage. This vehicular parking will also provide a space for the rectory and its priests as employees and/or a three (3) bedroom residence. Because of the small size of the classrooms being added (conference room sized), and the large percentage of access space in each (walkways, door swings, etc.) they can be vehicular parked more efficiently than the larger more typical classrooms the code was written to accommodate. Approximately eighteen (18) of the stalls will be reserved for Newman Center/University of Mary Staff and ten (10) for their guests/visitors. These will not likely be gated, but may be metered or otherwise limited. On weekends, the Newman Center has made arrangements to park in adjacent facilities and the stalls in the parking garage will be used as handicapped and special access stalls for groups using the church facilities. There will be no vehicular parking south of the alley. All of the code required bicycle parking required for the uses south of the alley will be accommodated on-street in the space between the back of curb and the face of building.

The rest of the vehicular parking on the ground level, lower level and second level will provide 100% of the code required vehicular parking for the residential guests and all of the parking required by code for the commercial uses. It is anticipated that the College Avenue frontage on the first and second levels will be retail and restaurant uses with office on the small third level. On the 7th Street façade, the ground floor is anticipated to be retail with some restaurant uses and office uses above. The percentage of each of these commercial uses may be adjusted based on market acceptance of these uses at this location. The percentage of semi-public parking spaces that are for guests and visitors or for employees will also adjust based on the type of use with retail or restaurant being heavy on guests/visitors and office being heavy on reserved for employees. While the commercial uses in the vehicular parking code are allowed to freely adjust, they have separate bicycle parking requirements that must and will also be met in the on-street parking in the area between back of curb and face of building. Because of the dramatically enhanced bike parking being provided for the residents (1 space/bed), additional guest bike parking will not be provided for the residential uses. At least thirty-seven (37) on-street bicycle parking spaces will be provided between the back of curb and face of building to accommodate all of the commercial uses north of the alley.

Because of the close proximity to the ASU campus and the issues it creates with parking in the area, all spaces in the garage will be reserved or metered, limited to support the uses on Site, and therefore not truly public, but called out here as "semi-public" for the commercial and classroom spaces. The vehicular parking stalls permanently reserved for residences are not part of the semi-public spaces. Building management will tow the vehicles of users who have parked in the garage, but are not associate with one of the uses on Property. Upper levels of the parking Garage are gated and reserved parking for residents and potentially the employees of the leased office or commercial space. The gates for the upper levels are within the garage and provide ample stacking length within the garage itself. An empty (striped out) vehicular parking stall will be provided outside the location of the gate so that vehicles that are not allowed to enter can turn around.

In total, the project will provide 217 of the code required 444 vehicular parking stalls for the new uses. As required by City Staff, none of the bicycle parking will be provided on the lower level, and the project in total will be providing 859 bicycle parking spaces, more than twice the required code. Of these bicycle spaces, 61 will be provided on-street for commercial uses and guests.

As previously described, the provided parking for the residential use is less than that required by code per the above explanation. The proposed reduced ratios for the residential use are summarized in **Table 5**.

Table 5: Proposed Residential Ratios

Unit	Number of Bedrooms	Parking Provided	Ratio
One-Bedroom	23	11.56	0.5/Bed
Two-Bedroom (partial students)	312	68.64	0.22/Bed
Three-Bedroom (live/work rectory)	3	0	0.0/Bed
Four-Bedroom (students)	460	0	0.0/Bed
TOTAL	798	80	

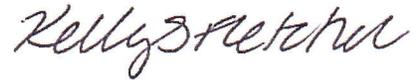
CONCLUSIONS

- At full build-out, the project will consist of 295 apartment units (798 bedrooms), retail, restaurant, and office space.
- The proposed development will provide a total of 217 parking spaces. The total parking spaces is comprised of 10 on-street parking spaces for commercial and guests, 127 parking spaces in the garage for commercial and guests, and 80 parking spaces reserved for residents separated by a gate. The garage parking spaces designated for commercial uses will be controlled by the use of metering to discourage student residents from utilizing them.
- The project will be providing 859 bicycle parking spaces, more than twice required by code. Of these bicycle spaces, 61 will be provided on-street for commercial uses and guests.
- Per the City of Tempe City Center (CC) District requirements, 336 parking spaces are required for the residential use, and 108 parking spaces are required for the non-residential uses. The existing Newman Center office is being converted to storage, and the Newman Center has an agreement with the ASU Foundation to utilize their garage for parking.
- Because the student residents will have a reduced need for personal vehicles, limited parking has been provided for the residential use. Should the land use change in the future such as to a hotel, the lower level of the garage has a height of 15 feet allowing for automated parking to be installed if needed. Off-site parking would also be procured at the time if needed.
- The provided parking for the residential use is less than that required by code. The proposed reduced ratios for the residential use is 0.5 spaces per bedroom for one-bedroom units, 0.22 spaces per bedroom for two-bedroom units, 0.0 spaces per bedroom for the three-bedroom live-work unit, and 0.0 spaces per bedroom for the four-bedroom units.

Should you wish to discuss this information further, please contact me at (480) 659-4250.

Sincerely,

CivTech

A handwritten signature in black ink that reads "Kelly S. Fletcher". The signature is written in a cursive, flowing style.

Kelly S. Fletcher, P.E.
Project Engineer

ASU
ARIZONA STATE UNIVERSITY

October 3, 2005

Father Nathan Castle
All Saints Catholic Newman Center
230 E. University Avenue
Tempe, AZ 85281

Dear Father Nathan:

In response to your recent request, I would like to summarize the current agreement between Arizona State University and the All Saints Catholic Newman Center in Tempe regarding parking in the ASU Foundation Building parking structure:

- Newman Center patrons who do not have current ASU Foundation Building parking decals are required to pay for parking during the regular work week (Monday through Friday).
- On non-event weekends, the structure's gate arms are raised and parking is currently free. The Newman Center may utilize free parking for their early morning services, as they have done in the past.
- On ASU football (or other Sun Devil Stadium event) days, Newman Center patrons will not be charged provided they are out of the structure before event parking begins.
- If Newman Center patrons require parking during an ASU football game or other special event, coordination through the ASU Parking and Transit Services Events Office (480-965-6209) is required, and charges may apply.

We value our long-standing relationship with the Newman Center. Please feel free to contact me if you have any questions regarding this policy.

Sincerely,


Linda I. Riegel
Director

cc: Ira A. Jackson, President and CEO, ASU Foundation

MAIN CAMPUS
PARKING AND TRANSIT SERVICES
PO Box 875205, Tempe, AZ 85287-5205
(480) 965-6406 FAX: (480) 965-0712
E-MAIL: PARKSVCS@asu.edu

ATTACHMENT 128



CATHOLIC NEWMAN CENTER

SERVING ARIZONA STATE UNIVERSITY

13 March 2016

RE: Parking demand at the All Saints Roman Catholic Newman Center, Tempe Arizona

To whom it may concern,

In our efforts to incorporate a student housing component and additional office and classroom space into our ASU Newman Center Grounds, we have diligently examined our vehicular parking needs. We intend to move existing offices into the new facilities when completed, converting the existing space into a kitchen to service our social hall and storage space in the basement of the Historic Old Saint Mary's Church.

Newman staff work flexible hours and come and go at various time throughout the day and evening. Many are ASU students interning part-time at the Newman Center. Other staff members who provide counseling or marriage preparation only come in the evenings. Most of the staff work Monday through Thursday, and the Newman offices are closed on Fridays. The University of Mary currently has a staff of three (3) and teach classes each weekday for ASU students and in the evening for many living in the metro area. The only major event that goes on, on-site daily is the evening Mass, which is currently scheduled at 6:00 PM and lasts for approximately forty-five minutes. Many of the students and individuals attending work in the immediate surrounding area and can easily walk to mass.

The big events at the Newman Center occur on the weekends. Weddings are often scheduled in the Historic Church on Saturdays when ASU is not hosting a football game. On Sunday, the Newman Center currently holds five (5) masses which currently hold up to 400 parishioners, but with the new construction should be able to hold up to 600 parishioners as originally intended and designed.



CATHOLIC NEWMAN CENTER

SERVING ARIZONA STATE UNIVERSITY

Per our vast experience managing the existing private surface parking lot north of the alley for the last six (6) years, we know how many spaces are needed for the Newman Center staff and daily visitors. We rarely (if ever) have all of the staff and additional visitors on-site at the same time. As such, we typically lease out most of the existing parking spaces on an hourly or daily rate reserving five (5) to eight (8) for staff use and keeping three (3) to five (5) available for guests/visitors. We have spent time with all of our key staff members estimating the long term growth of the activities at the center. We have also had discussions about incorporating the University of Mary into the grounds as they are currently housed in an adjacent property. As a result of these efforts, we were able to inform the architect of our needs for office space, conference rooms, and small (conference room sized) classrooms in our new building south of the alley as well as the vehicular parking we will need in the project's garage. We anticipate that our needs will grow to twenty (20) daily spaces with eight (8) additional spaces for the University of Mary.

Approximately eighteen (18) of the stalls will be reserved for Newman Center/University of Mary Staff and ten (10) for their guests/visitors. This ratio would adjust throughout the day, with more visitors in the evening than staff. These do not have to be gated, but should be metered or otherwise limited. On weekends, we have made arrangements to park in the adjacent ASU Foundation Building as part of a long term agreement when they wanted to remove the generations old parking lot there and add the existing building and parking structure to their site. We would also like to use the Newman Center stalls in the parking garage on weekends when the staff (except the priests) are not on site as handicapped and special access stalls for groups using the church facilities.

Sincerely,

A handwritten signature in black ink that reads "Michele Kilker". The signature is written in a cursive style.

Michele Kilker, Director of Administration
All Saints Roman Catholic Newman Center Tempe

MAXWELL ON COLLEGE TRAFFIC IMPACT ANALYSIS

Southwest Corner of College Avenue and 7th Street
Tempe, AZ

Prepared for:

Gensler
201 East Washington Street, Suite 750
Phoenix, Arizona 85004

For Submittal to:

City of Tempe

Prepared By:



CivTech, Inc.
10605 North Hayden Road
Suite 140
Scottsdale, Arizona 85260
(480) 659-4250



Expires 12-31-2015

December 2015
CivTech Project No. 15-1470

EXECUTIVE SUMMARY

This report documents a traffic impact study prepared for the proposed 'The Maxwell on College' apartment development to be located in the southwest corner of College Avenue and 7th Street in the City of Tempe, Arizona. The project is proposed with 271 dwelling units consisting of 615 total bedrooms. The project will also have 6,000 square feet of retail space. A proposed garage entrance is proposed on 7th Street and the adjacent alley. CivTech Inc. has been retained to complete a traffic impact study for the proposed development during the planning process.

The purpose of this study is to address traffic and transportation impacts of the proposed development on the surrounding streets and intersections. This was prepared to standard criteria set forth by the City of Tempe in their *Guide for the preparation of Transportation Impact Studies, updated 05/2014*. The specific objectives of the study are:

1. Evaluate lane requirements on all existing roadways and at all existing intersections within the study area.
2. Determine future level of service for all proposed major intersections within the study area and recommend any capacity related improvements.
3. Determine necessary lane configurations at all major intersections within the proposed development to provide acceptable future levels of service.
4. Evaluate the need for future traffic control changes within the proposed study area.
5. Evaluate the need for auxiliary lanes at stop and signal controlled intersections.

This study evaluates the existing year (2015) and opening year (2017). For purposes of this study, it was assumed that full build-out of the proposed development will occur in 2017.

The following conclusions and recommendations have been documented in this study:

- The project is proposed with 271 dwelling units consisting of 615 total bedrooms per the proposed site plan. The project will also have 6,000 square feet of retail space. The projected build-out year assumed for this analysis is 2017.
- The proposed development is anticipated to generate 1,140 external daily trips with 57 trips occurring during the AM peak hour and 97 trips occurring during the PM peak hour. Due to the proximity of the site to ASU's main campus, it is expected that many of the residents will be college students. Therefore, trip generation for the proposed apartments was estimated utilizing similar student housing rates that have been applied within studies for other apartment developments in Tempe. These rates are derived from a *Private Student Housing Apartments Memo*. Generated trips for the retail use were estimated for the proposed development utilizing the data given in the Institute of Transportation Engineers (ITE) *Trip Generation Manual, 9th Edition*.

- Access A: Access A on 7th Street is a full movement driveway that will provide access to the proposed garage and service access. The driveway entrance will be designed per Tempe's detail T-320.
- Access B: Access B on the adjacent alley is a full movement driveway that will provide access to the proposed garage. The driveway entrance will be designed per Tempe's detail T-320.
- Bicycles and Pedestrians: The proposed development will have a high volume of pedestrian and bicycle activity due to its close proximity to ASU. Bike racks are provided in the northeast corner of the development. The site plan also includes bike storage within the parking garage. There are existing bike lanes on College Avenue. The proposed sidewalks along the 7th Street and College Avenue frontages tie into the existing pedestrian facilities and are pedestrian friendly with ample landscaping and benches. Because the development is being designed with pedestrians and bicyclists in mind and will provide sidewalks that preserve the character of downtown Tempe, pedestrian and bicyclists will not be adversely affected.
- Right-turn deceleration lanes are not warranted at the proposed driveways.
- In horizon year 2017, all study intersections are expected to have an overall acceptable level of service (LOS D) or better.
- The model for this traffic impact study resulted in zero vehicles from 'The Maxwell on College' development making the southbound right-turn movement from College Avenue onto University Avenue. Per the model, only 5 vehicles from 'The Maxwell on College' development are anticipated to make a southbound left-turn at the College Avenue/University Drive intersection. Because the access points are on 7th Street and the Alley and not on College Avenue, most vehicles heading west would avoid College Avenue. The 'The Maxwell on College' student housing project would have minimal impact to the southbound approach at the College Avenue/University Drive intersection. Constructing a southbound right-turn lane at the College Avenue/University Drive intersection would require the removal of decorative pavement, landscaping, and would change the existing crosswalk that provides direct access to ASU. The crosswalk would need to be moved west with the construction of a right-turn lane.

PUBLIC INVOLVEMENT FINAL REPORT

The Maxwell on College – All Saints Catholic Newman Center – University of Mary

Project Address:

712 South College Avenue
Tempe, Arizona 85281

Cross Streets:

College Avenue and University Drive

Project Location: Northwest corner of University Drive and College Avenue

Parcels: 132-27-356
132-27-107A
132-27-110
132-27-111
132-27-112
132-27-113

Case Number: PL150419

16 November 2015

NOTIFICATION AND POSTING DATES

Signs were installed (posted) in locations as directed by the Project Planner, Karen Stovall beginning on 24 November 2015 with the text approved in the PIP (see Exhibit E-Affidavit of Sign Posting). On the same day, letters were sent to all property owners of record within 600' via U.S. Mail as well as to the chairperson registered neighborhood association (NA) and homeowners association (HOA) within 1320' of the property. See a copy of the letter sent in Exhibit C. – Copy of Notification Letter. In addition, as directed by City Staff, the same notification letter was sent to ASU via Angela Creedon.

MAP OF NOTIFICATION AREA

All property owners of record within 600' (see Exhibit A – Parcels within 600' (map) and Exhibit B – Parcels within 600' (list)) of the project were informed by letter via U.S. Mail of the neighborhood meeting that was hosted in the Newman Center's Social Hall on the evening of December 9th, 2015. One letter was sent to each different property owner or mailing address provided by Maricopa County's parcel map. Three letters were returned as un-deliverable.

LIST OR REGISTERED NEIGHBORHOOD AND HOMEOWNERS' ASSOCIATIONS NOTIFIED

Letters were sent to the chairperson registered neighborhood association (NA) and homeowners association (HOA) within 1320' of the property (see Exhibit D – NAs and HOAs within 1320').

DATES AND LOCATIONS OF MEETINGS WHERE CITIENS WERE INVITED TO DISCUSS THE PROPOSAL

Neighborhood Meeting (required)	December 9 th , 2015	Newman Center's Social Hall – 230 E University Dr
Parish Meeting	November 22 nd , 2015	Newman Center's Sanctuary – 230 E University Dr
Private Meeting for neighboring Tenant	November 25 th 2015	Newman Center's Social Hall – 230 E University Dr
Private Meeting for Neighbor	December 2 nd 2015	Newman Center's Social Hall – 230 E University Dr
Private Meeting for Neighbor	December 7 th 2015	Newman Center's Social Hall – 230 E University Dr
Private Meeting for Neighbor	December 9 th 2015	Newman Center's Social Hall – 230 E University Dr
Private Meeting for Neighbor	December 14 th 2015	Neighbor's Place of Business – College Avenue

INDIVIDUALS NOTICED AND PARTICIPATED

Because of the high visibility and traffic along College Ave and University Drive where signs were posted advertising the neighborhood meeting, it is unclear how many individuals were noticed, but clearly the number is in the thousands of individuals. Five (5) people not directly involved with the project attended the Neighborhood Meeting who all found out about the meeting via the posted signs. Private meetings were scheduled with stakeholders (neighbors and tenants) including eight (8) additional individuals. Finally, a meeting was held primarily for parishioners which was attended by approximately fifty (50) individuals. We continue to reach out to neighbors and stakeholders, but will not be including those in this final report as it is due five (5) days after the required neighborhood meeting.

NEIGHBORHOOD MEETING FORMAT

As requested by City Staff, the format of the neighborhood meeting was in the "Preferred Format" with a group presentation followed by citizen questions and responses. At the Newman Center, the Social Hall has an entrance from the central courtyard lawn, inside this entrance a table was be set up with sign in sheets. Chairs were provided for attendees to sit in. The meeting included an introduction and a presentation of the project

with images provided by image boards and via electronic presentation. Members of the project team stayed after the presentation to answer any individual questions that were posed by attendees.

STAKEHOLDER ISSUES OR CONCERNS

Below is a list stakeholder issues or concerns expressed by the participants.

STAKEHOLDER ISSUE OR CONCERN	ADDRESSED?
What does ASU think?	ASU has been contacted, meetings have occurred and a formal opinion of the project has not been provided to date.
Will shopping remain along College Avenue?	The College Avenue frontage will be lined with ground level retail.
Height and Massing along College Avenue	The height is substantially less than the existing entitlement. Reducing the height further is not cost effective in a steel, glass and concrete structure and Council has directed that wood framed structures for multi-family housing are in appropriate in this character area. The massing along College Avenue has been minimized by arranging the tower portions of the project in an east-west orientation.
How can we integrate the student housing into ASU's student housing programs?	We will continue to work with ASU on this topic.

SUBMISSION

This Public Involvement Final Report is hereby provided to Karen Stovall as required.

PUBLIC INVOLVEMENT FINAL REPORT
Newman Center/The Maxwell on College

Project Address:
712 South College Avenue
Tempe, Arizona 85281

Cross Streets:
College Avenue and University Drive

Project Location: Northwest corner of University Drive and College Avenue

Parcels: 132-27-356
132-27-107A
132-27-110
132-27-111
132-27-112
132-27-113
132-27-108A

Case Number: PL150419

14 March 2016

NOTIFICATION AND POSTING DATES

Signs were installed (posted) in locations as directed by the Project Planner, Karen Stovall beginning on 15 February 2016 with the text approved in the PIP (see Exhibit E-Affidavit of Sign Posting). On the same day, letters were sent to all property owners of record within 600' via U.S. Mail as well as to the chairperson registered neighborhood association (NA) and homeowners association (HOA) within 1320' of the property. See a copy of the letter sent in Exhibit C. – Copy of Notification Letter. In addition, as directed by City Staff, the same notification letter was sent to ASU via Angela Creedon.

MAP OF NOTIFICATION AREA

All property owners of record within 600' (see Exhibit A – Parcels within 600' (map) and Exhibit B – Parcels within 600' (list)) of the project were be informed by letter via U.S. Mail of the neighborhood meeting to be hosted in the Newman Center's Social Hall on the evening of March 2nd 2016. One letter was sent to each different property owner or mailing address provided by Maricopa County's parcel map. One letter was returned as un-deliverable.

LIST OF REGISTERED NEIGHBORHOOD AND HOMEOWNERS" ASSOCIATIONS NOTIFIED

Letters were also be sent to the chairperson of registered neighborhood association (NA) and homeowners association (HOA) within 1320' of the property (see Exhibit D – NAs and HOAs within 1320').

DATES AND LOCATIONS OF MEETINGS WHERE CITIZENS WERE INVITED TO DISCUSS PROPOSAL

Private Meeting for Neighbor	March 1 st , 2016	Newman Center's Social Hall (West) – 230 E University Dr
Neighborhood Meeting (Required)	March 2 nd , 2016 5:30p	Newman Center's Social Hall (West) – 230 E University Dr
Parish Meeting	March 2 nd 2016 6:45p)	Newman Center's Social Hall (West) – 230 E University Dr

INDIVIDUALS NOTICED AND PARTICIPATED

Because of the high visibility and traffic along College Ave and University Drive where signs were posted advertising the neighborhood meeting, it is unclear how many individuals were noticed, but clearly the number is in the thousands of individuals. Four (4) people not directly involved with the project, but members of the church, attended the Neighborhood Meeting who all found out about the meeting via announcements at weekend masses. A private meeting was scheduled with a stakeholders (neighbor) including two (2) additional individuals. Finally, a meeting was held primarily for parishioners at which there was no attendance. We continue to reach out to neighbors and stakeholders.

NEIGHBORHOOD MEETING FORMAT

As requested by City Staff, the format of the neighborhood meeting was the "Preferred Format" with a group presentation followed by citizen questions and responses. At the Newman Center, signs pointed the way to the meeting in the Social Hall inside a table was be set up with sign in sheets. Chairs were be provided for attendees to sit in. The meeting included an introduction and a presentation of the project with images projected on the large wall. Members of the project team stayed after the presentation to answer any individual questions that may be posed by attendees.

STAKEHOLDERS ISSUES OR CONCERNS

Below is a list of Stakeholders issues or concerns expressed by participants.

STAKEHOLDER ISSUE OR CONCERN	ADDRESSED?
When will rooms be available?	Fall 2018, may be a soft opening of one of the towers the semester before
What sustainable elements are being incorporated?	Likely LEED Silver level, but may pursue other certification for sustainability.
Have you thought about affordability of the units?	The units will start out at the top of the market because of Tempe's requirements in architecture and other elements for such buildings. This will ensure the building will last for a century as the rental prices normalize.
Is there room to include a Montessori school? The students could volunteer there and we need to have young children in the mix.	Newman Center's mission is to ASU. While we do serve some alumni and teachers with young children, it is not our primary goal, and as such we have partnered with Our Lady of Mount Carmel to provide educational services to our young children.
Have you considered adding art to the alley?	Yes, we are currently working with an ASU artist and Tempe's Economic Development Office to pursue such options.
You should add gardens and make the students work them.	We are looking at the possibility of incorporating Agriscaping into the Sky Park, and can offer the opportunity for students to participate.
The buildings are too tall.	ASU's master plan and the City's Design Guidelines for downtown both express that this is an area that is appropriate for this type of height. Student housing on this type of urban, privately held land requires enough units to attract a quality national operator as well as an economically viable project.
Do not believe this is the appropriate location for students.	ASU is currently building student housing for 1600 freshmen engineering students on the other side of the Foundation Building parking structure at a bed/acre ratio at or above ours, and at eight stories
Do not think there is enough parking provided.	We are accommodating all of the commercial and guest code required parking and are parking 29% of the residential units as is required by the CC parking code. We are providing more than twice the required bicycle parking.
What are the next steps through the process	Tentative hearing dates provided. Walked through the approval process.

SUBMISSION

This Public Involvement Final Report is hereby provided to Karen Stovall as required.

ACTION SUMMARY



On January 21st, 2016, the Tempe Historic Preservation Commission considered a certificate of appropriateness request for **THE MAXWELL (HP121015A / PL150419)**, a proposed mixed-use development containing 231 dwelling units, restaurant, retail, office, classroom, and church uses. The proposal includes redevelopment of portions of the Tempe Historic Property Register-listed St. Mary's Church / Our Lady of Mt. Carmel Catholic Church parcel, but does not entail alteration of the 1903 church building. Staff recommended conditional approval of the request certificate of appropriateness.

Trevor Barger of Espiritu Loci presented on behalf of the applicant. Following Barger's presentation, staff expressed conditional support for the request. Audience and Commission questions and discussions followed, with some concern voiced regarding the proposed College Avenue encroachment building space limiting views of Tempe (Hayden) Butte when looking north from University and the historic church steeple when looking south down College Avenue. Staff advised the Commission that their jurisdiction was limited to the designated parcel. As the College Avenue encroachment is proposed for what is currently a separate parcel, Historic Preservation Commission action could not address that aspect of the development proposal. After further discussion and consideration, the Historic Preservation Commission voted in favor of the following motion relating to agenda item three (Request for a certificate of appropriateness approving proposed redevelopment of portions of the **St. Mary's Church / Our Lady of Mt. Carmel Catholic Church** parcel for THE MAXWELL, located at 712 South College Avenue. The applicant is Tony Wall of Maxwell Tempe, LLC.):

MOTION [PROPER]: Move to approve applicant's certificate of appropriateness request, contingent upon completion of HPO-recommended condition regarding protection plan. SECOND [SOLLIDAY]; APPROVED, 7-0.

The condition of approval, as recommended in the staff report and approved by a unanimous vote of the Historic Preservation Commission, is as follows:

1. A protection plan detailing the means by which structural damage to the 1903 church building resulting from construction activities will be prevented is to be submitted to the Tempe Historic Preservation Officer. Said plan must be approved by the Tempe Historic Preservation Officer before construction activities commence.


John Larsen Southard, Tempe Historic Preservation Officer

January 25th, 2016

CHARLEY FREERICKS



Karen Stovall, Planning Division
City of Tempe Community Development Department
31 East Fifth Street
Tempe, Arizona 85281
Via email: karen_stovall@tempe.gov; cc: trevor@espirituloci.com

RE: **NEWMAN CENTER / THE MAXWELL ON COLLEGE (PL150419)**, located 712 South College Avenue (College Avenue and 7th Street).

Dear Ms. Stovall,

This letter is in support of the Newman Center/The Maxwell on College proposed development. As long as I can remember (my first visit to ASU was spring 1977) Old St. Mary's and the Newman Center has been a part of the Downtown Tempe experience. As Tempe has evolved into Arizona's most urban city, we have all watched the Newman Center "shrink" physically as the skyline blooms around it. Now it is very exciting to see a plan for the property to expand and evolve to maintain its prominence in ever-evolving Tempe.

ASU has always held something for almost anyone, and having a faith-based center is very important to some ASU stakeholders. Allowing the Newman Center/The Maxwell project to proceed will create an excellent opportunity to expand the services provided by one of Tempe's longest faith-based missions and will also allow the Newman Center to maintain its presence while growing with the modern city fabric. Similar to the housing options ASU provides for its academic groups, The Maxwell will allow students with a common focus the ability to reside together with their program and expand the fellowship opportunities afforded by a residential setting.

I have reviewed the presentation materials and the concepts designed by Gensler and find the project a fitting compliment to Tempe's skyline. Similar to adding a mix of residential and commercial uses at Centerpoint, like we did at DMB almost 15 years ago, adding residential, retail, and dining at this site will bring amazing new energy to College, complimenting the existing and new uses. The project will also bring an emphasis for healthy living alternatives which will be a strong addition to the neighborhood.

Tempe and ASU have embraced building height and density as great methods to grow the Tempe urban core. This proposed project will add to the built environment, providing great density, virtually on the main campus, al just a short walk to Downtown Tempe, ASU, light rail, and all Tempe/ASU have to offer. The proposal to add a dormitory to the properties just to the north of the Newman Center and increase the size of the dormitory from one, 432 bed, 270' high, 22-story tower with limited parking to two student housing towers (18 and 20 stories), 245' high with 795 beds, and commercial uses along street frontages is very exciting. I encourage Tempe and ASU to support this proposal enthusiastically.

Respectfully,

Charley Freericks
(Former president of DMB, developers of Centerpoint)



April 27, 2016

Ryan Levesque
Deputy Community Development Director
City of Tempe

Delivered via email: ryan_levesque@tempe.gov

RE: Maxwell Development

Dear Mr. Levesque:

It has come to our attention that the attached letter dated October 3, 2005 to the All Saints Newman Center has been used as a supportive tool for a student housing development at College and University identified as the "Maxwell." I am writing to advise city staff that this letter is not valid for those purposes and pertains only to parishioners of the All Saints Newman Center as described. At this time, ASU is unable to meet the parking needs of additional student and private residential development on College Avenue. As parking demands increase with each proposed development, the Maxwell and future developments in close proximity will need to meet their own parking requirements as ASU parking will be limited in nature.

Sincerely,

A handwritten signature in black ink, appearing to read "John P. Creer", with a long horizontal flourish extending to the right.

John P. Creer
AVP, Real Estate Development
Arizona State University

Cc: Morgan Olsen
Angela Creedon

UNIVERSITY REAL ESTATE DEVELOPMENT
PO Box 873908, Tempe AZ 85287-3908
Phone (480) 727-7676 Fax (480) 727-6210

ASU
ARIZONA STATE UNIVERSITY

October 3, 2005

Father Nathan Castle
All Saints Catholic Newman Center
230 E. University Avenue
Tempe, AZ 85281

Dear Father Nathan:

In response to your recent request, I would like to summarize the current agreement between Arizona State University and the All Saints Catholic Newman Center in Tempe regarding parking in the ASU Foundation Building parking structure:

- Newman Center patrons who do not have current ASU Foundation Building parking decals are required to pay for parking during the regular work week (Monday through Friday).
- On non-event weekends, the structure's gate arms are raised and parking is currently free. The Newman Center may utilize free parking for their early morning services, as they have done in the past.
- On ASU football (or other Sun Devil Stadium event) days, Newman Center patrons will not be charged provided they are out of the structure before event parking begins.
- If Newman Center patrons require parking during an ASU football game or other special event, coordination through the ASU Parking and Transit Services Events Office (480-965-6209) is required, and charges may apply.

We value our long-standing relationship with the Newman Center. Please feel free to contact me if you have any questions regarding this policy.

Sincerely,


Linda I. Riegel
Director

cc: Ira A. Jackson, President and CEO, ASU Foundation

MAIN CAMPUS
PARKING AND TRANSIT SERVICES
PO BOX 875205, TEMPE, AZ 85287-5205
(480) 965-6406 FAX: (480) 965-0712
E-MAIL: PARKSVCS@asu.edu

03 May 2016

Karen Stovall
Planning Division
City of Tempe Community Development Department

Delivered via email: karen_stovall@tempe.gov

RE: Letter from ASU dated April 27, 2016 regarding Maxwell Development

Dear Ms. Stovall,

Thank you for forwarding the letter from Arizona State University (ASU) dated April 27, 2016 regarding our project from John P. Creer to Ryan Levesque. We agree with Mr. Creer's statements regarding the earlier letter dated October 3, 2005. Per the parking study we submitted, and the parking understanding with ASU, the project (Newman Center) will continue using the parking at the Foundation Building for weekend mass and church event attendees (parishioners of the All Saints Newman Center) per the terms of the letter. The Newman Center/The Maxwell on College project does not propose using any ASU parking for its commercial or residential (student housing) uses.

Sincerely,

A handwritten signature in black ink, appearing to read 'Trevor Barger', written over a horizontal line.

Trevor Barger, AICP CUD
CEO

Cc: Ryan Levesque

Taken out of order Agenda Item #7:

MOTION: Motion made by Commissioner Spears to move up Agenda #7 **NEWMAN CENTER / THE MAXWELL ON COLLEGE (PL150419)** to the top of the agenda. Seconded by Commissioner Thornton

VOTE: Motion passes 7-0

Chairman Kent recused himself and Commissioner Langston joined the commission for the Public Hearing for Agenda #7 **NEWMAN CENTER / THE MAXWELL ON COLLEGE (PL150419)**

Appointment of a new Chairperson to conduct public hearing for Agenda Item #7:

MOTION: Motion made by Commissioner Killoren to appoint Commissioner Thornton as the chair for the public hearing for Agenda #7 **NEWMAN CENTER / THE MAXWELL ON COLLEGE (PL150419)**. Seconded by Commissioner Johnson

VOTE: Motion passes 7-0

The following items were considered for **Public Hearing:**

- 7) Request for a Planned Area Development Overlay and a Development Plan Review for a new four-story and 20-story mixed-use development containing 295 dwelling units, restaurant, retail, office, classroom, and church uses for **NEWMAN CENTER / THE MAXWELL ON COLLEGE (PL150419)**, located 712 South College Avenue. The applicant is Tony Wall, The Maxwell Tempe LLC.

PRESENTATION BY STAFF:

Karen Stovall, Senior Planner provided a brief description of the case including the location of the site, the history, current zoning of the property with aerial, elevations, and landscaping plans. Ms. Stovall pointed out several significant conditions of approval for this project. These include requiring the applicant to construct a southbound right turn lane on College Avenue and University Drive. Also, to provide a minimum of 444 parking spaces either on or off site as required by the code for the proposed residential uses as well as class rooms. Also, they will need an updated letter from the ASU Foundation regarding the 72 spaces required for the church use.

Commissioner Johnson asked Ms. Stovall if there were any renderings showing a right hand turn lane assuming what it would look like if constructed. Ms. Stovall replied no, but there is staff from the City's Transportation Division present to answer any questions.

Commissioner Spears asked if removing the 3 large palms would be done to create the right hand turn lane. Ms. Stovall replied that the palm trees would be removed and the sidewalk area would be shifted to the west to provide area for the right hand turn lane. Commissioner Spears asked if it could reduce the area where pedestrians would wait for the light to cross the road. Ms. Stovall replied yes.

Julian Dresang, City Traffic Engineer, explained that a right hand turn lane is needed at this location to accommodate vehicular traffic at an intersection that already has heavy pedestrian traffic throughout the day, in addition to the public transit related traffic. College Avenue is a major route for the public transit traffic to the Transportation Center. The green light time at this intersection is mostly used by pedestrians and doesn't allow for much vehicular traffic to move. As a result of this project, the pedestrian traffic will increase greatly. Mr. Dresang explained that moving the curb line back is actually moving it to its original location from the 1980's. While this addition will reduce the curb line, there will still be adequate pedestrian space for crossing. Mr. Dresang stated that the palms would be relocated with the construction of the right turn lane.

Commissioner Spears stated that she her concern that adding the right turn lane will not alleviate traffic but rather slow traffic down further and why a "pedestrian only" crossing signal is not being considered. Mr. Dresang advised

that a pedestrian only crossing, which is typically referred to as a pedestrian scramble, is not recommended at this intersection because the signal timing for pedestrian scramble adds to signal timing and significantly increases the wait time for vehicular traffic while the pedestrian traffic scramble cycle is in operation. This kind of signal is only effective where there is a high pedestrian crash rate. Chair Thornton asked Mr. Dresang if he had a count of how many pedestrians actually cross at that intersection hourly or daily. Mr. Dresang stated that during the peak hours, i.e., from 4pm to 4:15pm there are 1800 pedestrians and 4:15pm to 4:30pm there are 1500 pedestrians allowing about 3 vehicles to turn. This is because pedestrians are still crossing while the green signal is on for vehicular traffic to move. Currently, all the green time is taken by the pedestrian leaving only the orange and red signal time for vehicles to turn either left or right at this intersection.

Commissioner Spears disagrees with separating right and left turn lane would be a solution to moving traffic along because the students are on both sides of the streets. Mr. Dresang advised that having the right hand turn lane would provide relief to the vehicles turning right because currently all vehicles regardless of whether they are turning left or right are stopped at the intersection because of the pedestrian traffic.

Commissioner Brown asked there are plans in the future to close College Ave to vehicular traffic. Mr. Dresang stated that the City does not have any plans in the master transportation plan to close College Avenue and make it pedestrian only because this is all an important road to access the City's Transit Center.

Commissioner Lyon asked if there is data to show that a pedestrian scramble won't work that well. Mr. Dresang advised he did do some research on pedestrian scramble in other states and in the past the City had looked at the pedestrian scramble at this location but determined it was not going to help move the traffic efficiently.

Commissioner Killoren asked why would there be a condition for a right hand turn lane if the Maxwell project states that there would be zero vehicular trips that would be generated as a result of this project. Mr. Dresang responded that the additional bicycle and pedestrian traffic generated from this project will still have an impact on College Avenue and this intersection and will add to the existing traffic congestions.

Chair Thornton clarified that there are about 1200 pedestrians to 3 cars. Mr. Dresang confirmed that only three vehicles are able to move and only at the orange signal due to all the green time being taken by the pedestrian traffic.

Commissioner Johnson asked if the City has looked into allowing "left turn only" or "right turn only" as an option at this intersection. Mr. Dresang responded that the goal is to maximize the utilization of the intersection and not limiting it. Commissioner Johnson asked if there is any data available for number of right and left turns at this intersection. Mr. Dresang stated that per the traffic study provided by the applicant, at the build-out of this project in 2017, there would be 38 cars for right turn and 54 cars for left turn projected at the am peak and 94 right turn and 117 left turn projected during pm peak.

PRESENTATION BY APPLICANT:

Trevor Barger, Applicant for the projected, provided a presentation that included describing the architecture, enhancing the College Avenue experience, and keeping with the historic significance and architecture of the church. He continued to describe building design, elevations, site views, and location plans which include parking for vehicles and bicycles and consideration for pedestrian crossings.

QUESTIONS FROM THE COMMISSION:

Commissioner Killoren asked Mr. Barger what he proposes instead of the right hand turn lane. Mr. Barger stated that he has explored many different options such as closing College to vehicles and having it only available to pedestrians or have a right hand turn only lane during peak hours.

Chair Thornton asked Mr. Barger to confirm if the rationale behind not having parking spaces for the residents living in the 4 bedroom units is that these residents would not be allowed to bring their vehicle. Mr. Barger advised that it will be not be cost effective for a student to park in downtown.

Commissioner Lyon asked Mr. Barger to describe how the parking for this project will be managed and enforced. Mr. Barger stated that the intent is to have Downtown Tempe Authority (DTA) enforce parking. Parking in the garage for the 80 reserved spaces for the residents is going to be gated and access allowed only for those residents.

Commissioner Langston asked Mr. Barger for examples from downtown Tempe that have similar parking. Mr. Barger used University House as an example.

PUBLIC COMMENT: None

DISCUSSION BY THE COMMISSION:

Commissioner Lyon expressed that he did get a chance to speak with the developer of this project and go through the features and design. He stated that it is a very attractive project. His only reservation is with the parking. He appreciates the idea of more bicycles and pedestrian traffic. The second reservation he has is regarding the right turn lane and his concern is about getting only 3 cars through during the yellow light vs. 1200 to 1800 pedestrians during a cycle. He wants to know if it is possible to have a pedestrian scramble but change the timing to allow more cars through than pedestrians.

Commissioner Spears expressed that she appreciates the design and openness. She is concerned with the right turn lane and does not think it's going to accomplish anything. She thinks the concern is not the cars, but it is the pedestrians and the safety with the narrowing of the sidewalk. Commissioner Spears said that generally she will support the project.

Mr. Dresang and Mr. John Hoang, also from the City's Transportation Division, clarified the previous statement about the number of pedestrian traffic versus vehicular traffic count at that intersection and that a traffic signal cycle is 110 seconds which is 2 minutes per cycle which totals to 6 vehicles and 60 pedestrians per cycle.

Commissioner Killoren expressed that he supports the project but does not support the condition for the southbound right turn lane. He would like to remove it and that the developer will continue to work with staff to find a good solution for it. This is a quality of life issue. He also supports removing the condition on parking and believes in the logic that is applied to the parking spots.

Commissioner Brown expressed that he likes the project. Commissioner Brown asked if the brick being used is the same as the church. Mr. Jay Silverberg with Gensler replied yes it will be a medium size brick. Commissioner Brown expressed that he will support the project.

Commissioner Johnson expressed that he likes the incorporation of the brick and the connecting features between the buildings. He appreciates that there are more than one level of retail and commercial. Commissioner Johnson stated that he is conflicted with the right hand turn lane. He is concerned that reducing the existing width of the sidewalk for pedestrians and bicyclists is problematic. He hopes that there is a cleaner and better solution to resolve this issue. Commissioner Johnson also suggested that may be a different set of parking standards be created for student housing rather than using the current downtown parking standards. Commissioner Johnson does support the project and removing the two conditions as requested.

Chair Thornton expressed that she thinks it's a beautiful project and she is very excited about it. She agrees with the demographic that the developer is looking for. Chair Thornton thinks that having a right hand turn lane only and having pedestrians crossing on the left side seems like simple solution to the issue but she is not a traffic expert.

MOTION: Motion made by Commissioner Lyon and Seconded by Commissioner Killoren to recommend approval of the Planned Area Development Overlay and a Development Plan Review for **NEWMAN CENTER / THE MAXWELL ON COLLEGE (PL150419)**, with the following changes:

Remove stipulation #10 and #11c under Planned Area Development Conditions of Approval and stipulation #8 under Site Plan.

VOTE: Motion passes 7 - 0.

Stovall, Karen

From: [REDACTED]
Sent: Friday, May 06, 2016 10:19 AM
To: Stovall, Karen
Subject: ASU Newman Center/Maxwell Project

Council Members and members of City Planning Staff,

I wish to add my voice in support of the project for the ASU Newman Center and the Maxwell. As an active member of our community I support redevelopment of our land that supports downtown Tempe and more ability to house students together away from family centric established neighborhoods. Of the many recent projects for student housing I have been concerned about the resort feel and level of non academic amenities. But all the same, I support the developers who want to risk their money in the creation of new well designed buildings. I am intrigued with the concept of a faith based and inner faith model that this developer wishes to attempt. So then being that a glut of housing helps attain affordable rates, the question before you is more about the architecture of the building and the ability of our downtown to handle the increased population.

This is another point which I believe this development handles well. I like the renderings I have seen, with some private space, and much public space (and the depiction of opportunity for new small business models of commerce). I like that the design looks like it should fit in our modern but deeply historical downtown. Mill Avenue, the lake, the hip establishments (Tricks, Rula, Shady...) are all a fantastic blending. I think the architecture fits, and I believe Tempe has done a great job preparing for this very movement of population into the downtown.

Please consider the approval of this project.

Build Well,

Dave Maza
Remodel Tempe
[REDACTED]



May 6, 2016

Mr. David Nakagawara
Community Development Director
City of Tempe
31 East 5th Street
Tempe, AZ 85281

Via email to david_nakagawara@tempe.gov
and via Hand Delivery

Re: The Maxwell on College (PL150419)

Dear Dave:

I understand the Tempe City Council soon will consider a request by the Newman Center and Maxwell Tempe LLC to approve a Planned Area Development and a Development Plan Review for a new mixed-use development containing 295 dwelling units, a restaurant, retail, office, classroom and church space for the Newman Center/The Maxwell on College (PL150419) (the "Maxwell Project"). The purpose of this correspondence is to offer some feedback from Arizona State University (ASU) on the Maxwell Project application and to express some significant concerns about the intensity and density of this development. While ASU has some general concerns about placing almost 800 new student housing beds in this location, we are more concerned about the impact of the density and intensity of this development on College Avenue. As you know ASU supports and shares the City of Tempe's goal to increase the number of residents living in the downtown. Both ASU and the City of Tempe benefit from additional residents who are able to support businesses, work in downtown Tempe, engage with ASU, and contribute to the vibrant civic, sports and cultural amenities in the downtown Tempe area.

For downtown Tempe to be successful, the amenities in the downtown need to be more diverse than the outstanding bars and restaurants on Mill Avenue. It is for this reason that ASU and the City have spent the last ten years working closely to develop the section of College Avenue from University Drive on the south to Veteran's Way on the north into a pedestrian-friendly streetscape with specific design guidelines. The intent of the College Avenue master planning effort has been to help integrate the life of the University into the downtown core of Tempe. This section of College Avenue serves as both a gateway to the Tempe Campus and also as the heart of core downtown Tempe. Thus it is important that the City of Tempe and ASU work closely to make sure we understand the true impact of any proposed new development on College Avenue.

The height and density of the Maxwell Project proposal is out of scale and place with the surrounding buildings which frame College Avenue. We believe the intensity of a project on this site should reflect the adjacent buildings. The adjacent buildings include the Newman Center to

Business and Finance
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PO Box 873908, Tempe AZ 85287-3908
Phone (480) 727-7676 | Fax (480) 727-6210

the south, the six-story ASU Foundation Building and the five-story College Avenue Commons building. While we do not think that the Maxwell Project should be limited to the same height as the adjacent buildings, the proposed height of 20 stories is out of proportion and out of character with the existing neighborhood on the south end of College Avenue.

More importantly, the Maxwell Project proposal includes approximately 800 new student housing beds, with a minimum 217 new cars and 256 new bicycles that will feed onto and travel this section of College Avenue. The intersection of University Drive and College Avenue is already the busiest intersection on the Tempe Campus in terms of pedestrian and vehicle interaction. There are thousands of pedestrians who cross back and forth at the intersection of College Avenue and University Drive every day. With the addition of the traffic that would be generated by the Maxwell Project, there will be thousands of new people who use the intersection of University Drive and College Avenue. Before the City Council approves this project, we request that the City and the Applicant consider new pedestrian mitigation options to assure the safety of the pedestrians and vehicles crossing at the College Avenue and University Drive intersection. The staff report for the Maxwell Project states "...the models, pedestrian analysis and projections used in the TIA (Traffic Impact Analysis) do not quite match the existing conditions (volumes, pedestrian walking patterns, Level of Service) or realistically predict the future traffic patterns and conditions." Before any project is approved, we think it is important that all parties understand the existing pedestrian and vehicle conditions and the future impact of pedestrian and vehicle trips that may result from the Maxwell Project.

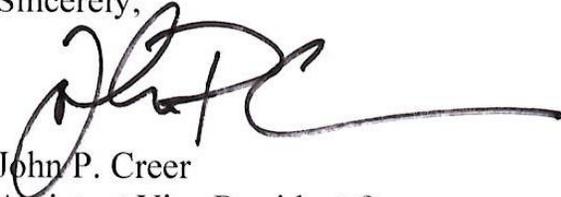
Earlier this year, the City of Tempe and ASU commissioned a joint traffic and parking study to evaluate the traffic and pedestrian patterns in and around the downtown Tempe area and the Tempe campus. The purpose of the study is to look at the Downtown Tempe/north Tempe campus area holistically and to determine the appropriate traffic and pedestrian mitigation measures that may be needed with the future developments at ASU and within the downtown Tempe core. This joint traffic study is not yet complete. Based on my conversations with the traffic consultants, I think they will have results in the next 60-90 days. Once we have the complete study, we will all be in a better position to review potential pedestrian and vehicle mitigation measures that would be appropriate for this type of proposal.

In the Maxwell Project Staff Report, the planning staff recommended several traffic mitigation conditions the City thought were important to address the vehicular and pedestrian traffic at College Avenue and University Drive. Unfortunately, the traffic mitigation conditions were removed by the Development Review Commission. At a minimum, we believe it was a mistake to remove the traffic mitigation conditions. More importantly, I think we can all agree that neither the City nor the Applicant understands the true impact of this development in terms on vehicle trips and pedestrian experience. The City and ASU are in the middle of conducting a joint traffic and parking study, which will help us determine the appropriate mitigation measures to help mitigate the impact of this development. Therefore, we respectfully request that the City Council continue this matter until the completion of the traffic study, so we can all understand the true impact of this development on College Avenue and University Drive.

David Nakagawara
May 6, 2016
Page 3

ASU and the City have worked together in partnership for over ten years to develop this section of College Avenue into a beautiful pedestrian boulevard with active streetscape. Before the Council approves any development, we should wait to review the joint commissioned study so we can all understand the true impact that a development of this proposed density and intensity will have on College Avenue.

Sincerely,

A handwritten signature in black ink, appearing to read 'John P. Creer', with a long horizontal flourish extending to the right.

John P. Creer
Assistant Vice President for
University Real Estate Development

Enclosure

cc: Dr. Michael M. Crow
Dr. Morgan R. Olsen
José A. Cárdenas

E S P I R I T U L O C I
The Spirit of the Place

20 May 2016

Ryan Levesque
City of Tempe, Development Services
31 E Fifth Street
Tempe Arizona 85280

Delivered via email to: ryan_levesque@tempe.gov

RE: Newman Center / Maxwell on College

Dear Ryan,

In our work to continuously improve the Newman Center / The Maxwell on College project, we have been working with City Staff to resolve issues, and addressing issues raised by others. Below is a summary of those efforts.

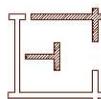
Issues raised by City Staff:

Right Turn Lane

We have been deeply involved in conversation to mitigate congestion issues at College and University. Several options were proposed. We have agreed with City Staff to a stipulation and graphic of the right turn lane to be added (see attached).

Parking

There was not time before the DRC Hearing to fully discuss the requirement of a set number of parking spaces, and the downtown parking code having no accommodations for student housing. After in depth conversation with Staff, and as there is no conversion for student housing or providing additional bike parking, we have agreed to park the project per code, with the required spaces for the existing church accommodated on an adjacent property. We are going to incorporate several elements to meet parking code which may include the automation of parking in the garage, additional parking in the garage, improving the efficiency of the parking in the garage (see attached) and on-street, reducing commercial space by converting it to rentable storage space or other less parking intensive uses, reducing the residential units or bedrooms, and/or acquiring additional off-site parking. None of these elements would change the exterior physical appearance of the project.



Issues raised by ASU (from letters by John Creer):

“The Maxwell and future developments in close proximity will need to meet their own parking requirements”

- Per the Staff issue above, we will be parking the project as required by City code.

*“**impact of the density and intensity of this development on College Avenue.**”*

ASU has added substantially to College Avenue intensity in the last couple years

- 3 restaurants,
- the ASU welcome center (beginning of all student tours),
- the Apple store,
- a book store and
- approximately 100,000 square feet of classroom space

Additionally:

- 300 feet east of College Avenue ASU is under construction with a 1594 bed freshmen dormitory
- Dec 11 ASU Master Plan update (pg 74) suggests adding 1560 residential units along College Avenue
- ASU has worked out an arrangement to close a portion of College Avenue to vehicular traffic for events

- The Newman Center is already approved for 432 bedrooms (180 units), the church and social hall already exist on site and the site is already home to 103 parking spaces and 18,000 square feet of commercial uses.
- The additional residential units and commercial space being proposed is not enough to noticeably impact the existing heavy use of College Avenue.
- Building design allows for the future closure of College Avenue to vehicular traffic if so desired by the City.

*“...**height and density of the Maxwell Project proposal is out of scale and place with the surrounding buildings.**”*

Height

- ASU is currently proposing a 20-story building on Myrtle (a few blocks away) on a site currently surrounded by buildings no taller than those on College Avenue.
- Height and density on adjacent ASU parcels is not limited.
- The ASU Foundation building when constructed was many times more massive than any building surrounding it, and noticeably tall than most buildings in the area.
- ASU was welcome to participate in the City of Tempe public, citizen process (2006) to set guidelines for development in the downtown that state that these parcels are appropriate for building heights up to 300' (see attached graphic)
- Several high-rise structures have been built, approved and proposed surrounding this site (see attached HEIGHT graphic)
- The two projects west of the site have both been approved for heights (235'-245') similar to those being proposed on this property (245').
- The existing zoning approval for the south half of the site is 270' (supported by ASU).

Density

- ASU Palo Verde Main (under construction) – 1594 beds @ 450 beds /acre
- ASU Manzanita Hall (recently remodeled and not repurposed as education uses) – 750 beds (was over 1000)
- ASU University Towers
- ASU future housing on College Avenue – 1560 residential units
- University House – 879 beds @ 482 beds / acre
- 707 S Farmer – 530 beds @ 1037 beds / acre
- Newman Center (existing approval) 432 bedrooms @ 502 / acre

Proposed Project – maximum 795 beds @ 420 / acre (**least dense of all projects in area**)

“...we do not think that the Maxwell Project should be limited to the same height as the adjacent buildings...”

- We agree.

“...consider new pedestrian mitigation options to assure the safety of the pedestrians and vehicles crossing at the College Avenue and University Drive intersection.”

- Several potential mitigation options were presented to City Staff (closing College Ave to non-municipal vehicular traffic, right turn only at intersection, pedestrian only traffic signal cycle (pedestrian scramble), no pedestrian crossing on the east side of the intersection (to allow easy left turns), add right turn lane). Only the addition of a right turn lane was an acceptable solution and is proposed as outlined above (see above and attached).

“At a minimum, we believe it was a mistake to remove the traffic mitigation conditions...”

- Right turn lane (mitigation condition requested by staff and removed by DRC) has been added as agreed to through discussion with City Staff (see above and attached).

“...continue this matter until the completion of the traffic study..”

- No clear information about scope of study, inputs provided, desired types of suggestions, firm completion deadline available at this time.
- No public input anticipated in this study
- No input requested or provided to the study by the DTA, DTA not involved in this study
- Increased traffic as a result of the additional units and commercial space is so minute as not to be a driving factor in traffic analysis
- Applicant cannot agree to a delay at this time with so little information about or involvement in the joint study.

Issues raised by City Council

View of “A” Mountain

- Several ASU Buildings currently block views of “A” Mountain from University Drive and ASU’s Campus:
 - Manzanita Hall
 - Palo Verde Main
 - Parking structures
 - Foundation Building
 - College Avenue Commons (CAC) Building
 - University Towers
 - Brickyard
- Tempe’s Marriot Residence Inn also blocks views of “A” Mountain
- Tempe’s street tree program on south College Ave has dramatically limited distant views of “A” Mountain
- No view easement or scenic easement exist over the site

Management Company
Asset Plus (see attached)

- Largest privately held student housing management company
- Highly response to “faith based” culture and willing to meet with Fr. Rob
- Great working relationship with ASU’s student housing management company (ACC)
- Managed similar urban, high-rise student housing projects successfully

ASU Student Housing Demand

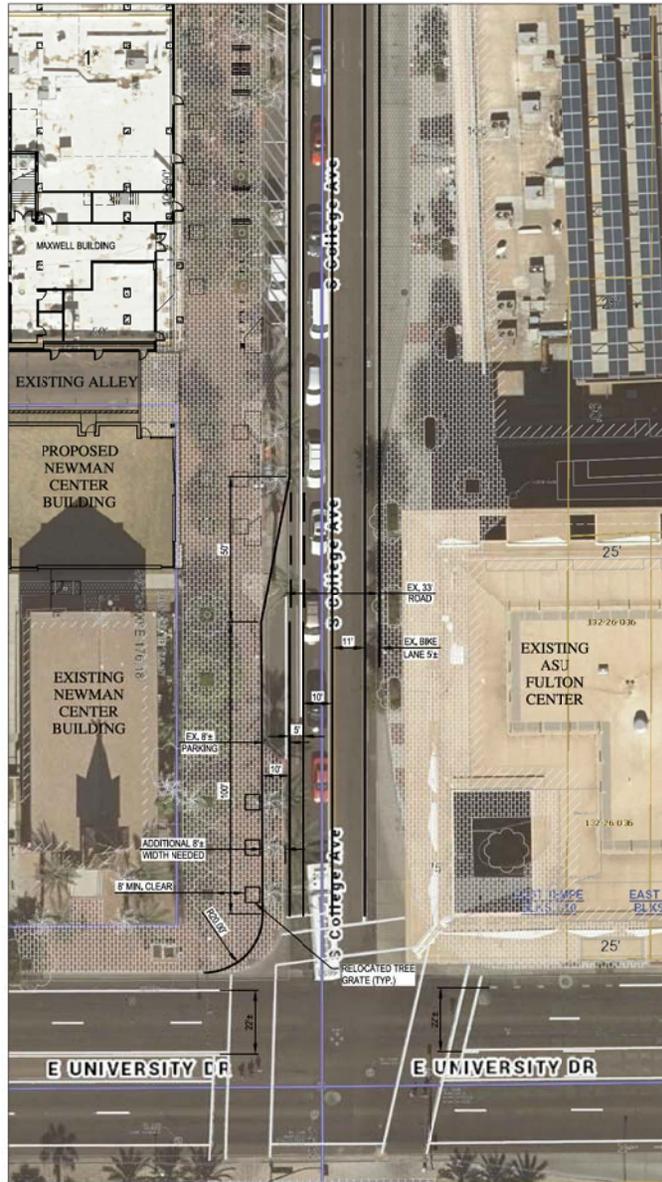
- statistics from Niche, ASU Tempe has 49,945 undergraduate students and currently provides housing for 9,106 of them
- ASU houses 70% of the freshmen, but only 19% of undergrads

I hope this information is helpful in the upcoming deliberation of our project.

Sincerely,



Trevor Barger, AICP CUD
CEO



The Maxwell
Tempe, LLC
712 S College Avenue
Tempe, AZ 85281

Gensler
201 East Washington St
Suite 750
Phoenix, AZ 85004
United States
Tel: 602.523.4900
Fax: 602.523.4949

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Date	Description
05-11-16	

Scale/Signature

Project Name
Newman Center / The Maxwell on College

Project Number
57.6119.000/ WP#154389

Description
RIGHT TURN LANE EXHIBIT

Scale
1" = 20'

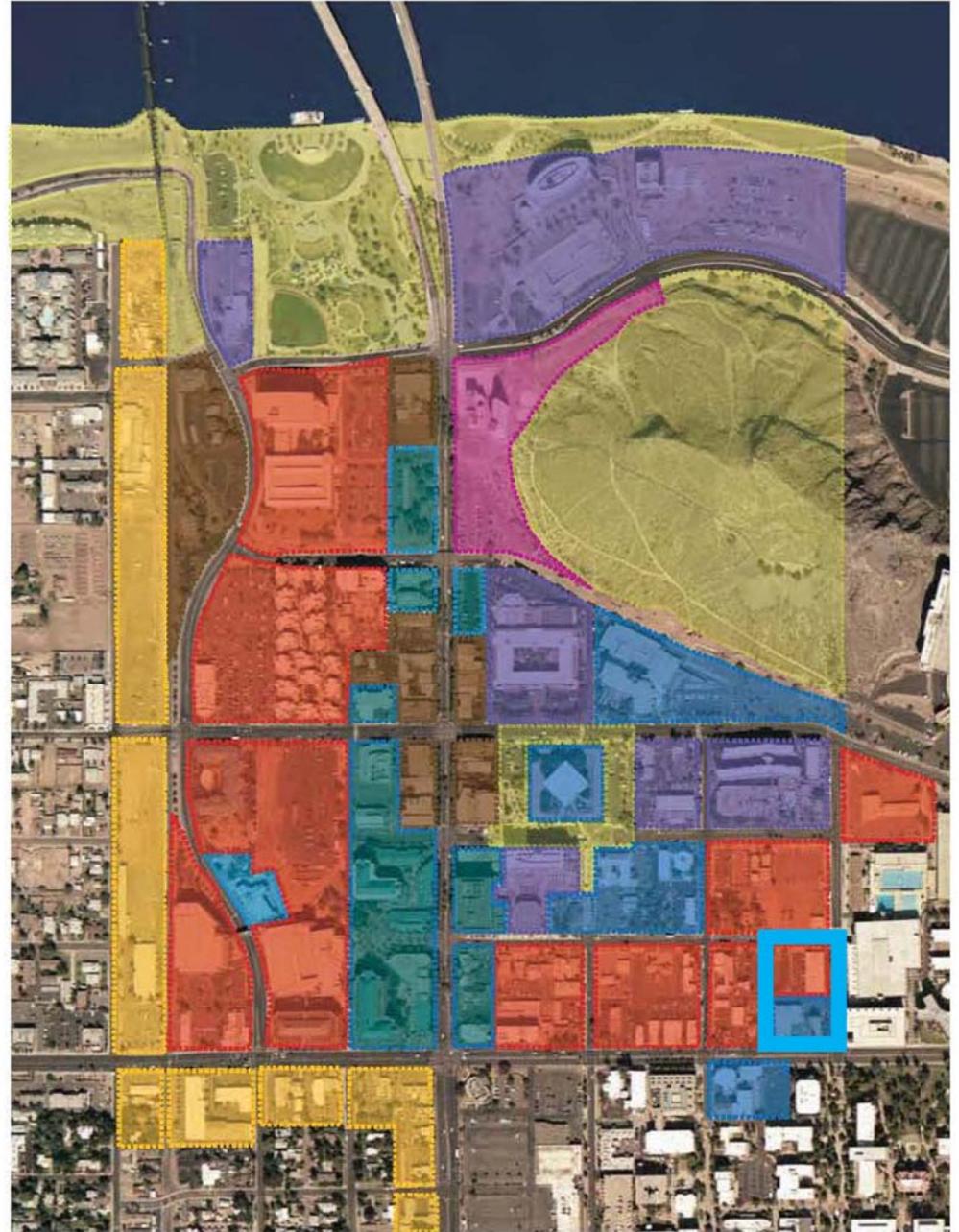
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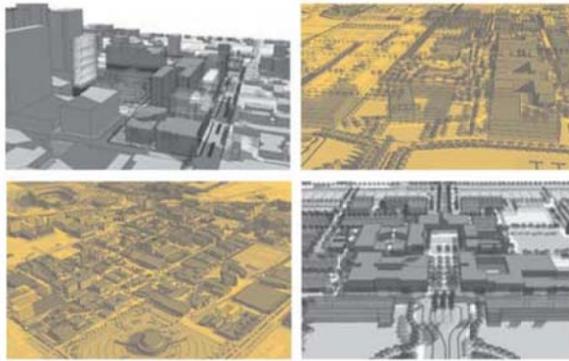




CONCEPT STUDY
DOWNTOWN BUILDING HEIGHTS
 Accepted 6 April 2006 (Central City Development Committee of the Whole)



* STEPBACKS are calculated as an average from street and/or park frontages, and pertain to primary building mass (additional articulation, utilizing balconies, canopies, etc., is not only encouraged, but expected).



Arizona State University

Master Plan Update

ASU ARIZONA STATE UNIVERSITY

DECEMBER 2011

Residential Development

In order to house 25% of projected student enrollment in on-campus facilities on land currently owned by ASU, the university must look at higher density development for new sites, and the redevelopment of older, low density halls in existing neighborhoods. Irish Hall, with 99 beds, averages 186 gsf per bed, a very low ratio by today's housing standards. It is a two-story building located across from the W. P. Carey School of Business and campus gateway to Cady Mall. Recent housing infill on the block has increased heights to four stories. The Irish Hall site is proposed for redevelopment into a new six-story dormitory. Hayden Hall is also a low density dorm, with only 151 gsf per bed. Depending on building condition, Hayden Hall may be a candidate for renovation to gain more square footage per bed.

Current planning and design for the Ocotillo housing development south of Apache Boulevard proposes 400 units in four-story buildings. The two-story single-family residential neighborhood south of Apache Boulevard requires some sensitivity in adjacent building height and massing. As a transitional site, university housing development here should maintain a lower height than other sections of campus.

Palo Verde Main is another low scale, underutilized housing site. At three stories, it is out of character with the height and density of Palo Verde East, West and Manzanita Hall. Redevelopment of Palo Verde Main as new six-story undergraduate dormitories would allow for modernization, increased square footage per bed, and an increase in the number of beds from 548 to over 800 (assuming 300 gsf per bed). Given the existing layout of the residential wings, construction could be staged in multiple phases.

The 10-acre Tempe Gateway site on Mill Avenue at University Drive provides a great opportunity to create a new, dense mixed use residential neighborhood, bridging campus and town. Twelve-story apartment towers with ground floor retail would bring a student population in close proximity to existing retail on Mill Avenue, helping support downtown businesses while still providing housing within an easy walk to the center of campus.

Increased residential density is more feasible north of University Drive. The verticality with "A" Mountain, Sun Devil Stadium, and the mid- to high-rise towers of Palo Verde East and West, and Manzanita Hall provide an existing visual context that can accommodate greater building height. Proximity to the light rail station and the Tempe Transportation Center on Veterans Way also provides an impetus for greater density to support transit ridership. Redevelopment of Block 12 and the Mona Plummer Aquatic Center on College Avenue can provide up to 1,560 residential units in ten-story buildings, integrated with future academic or administrative mixed use functions and street level retail.

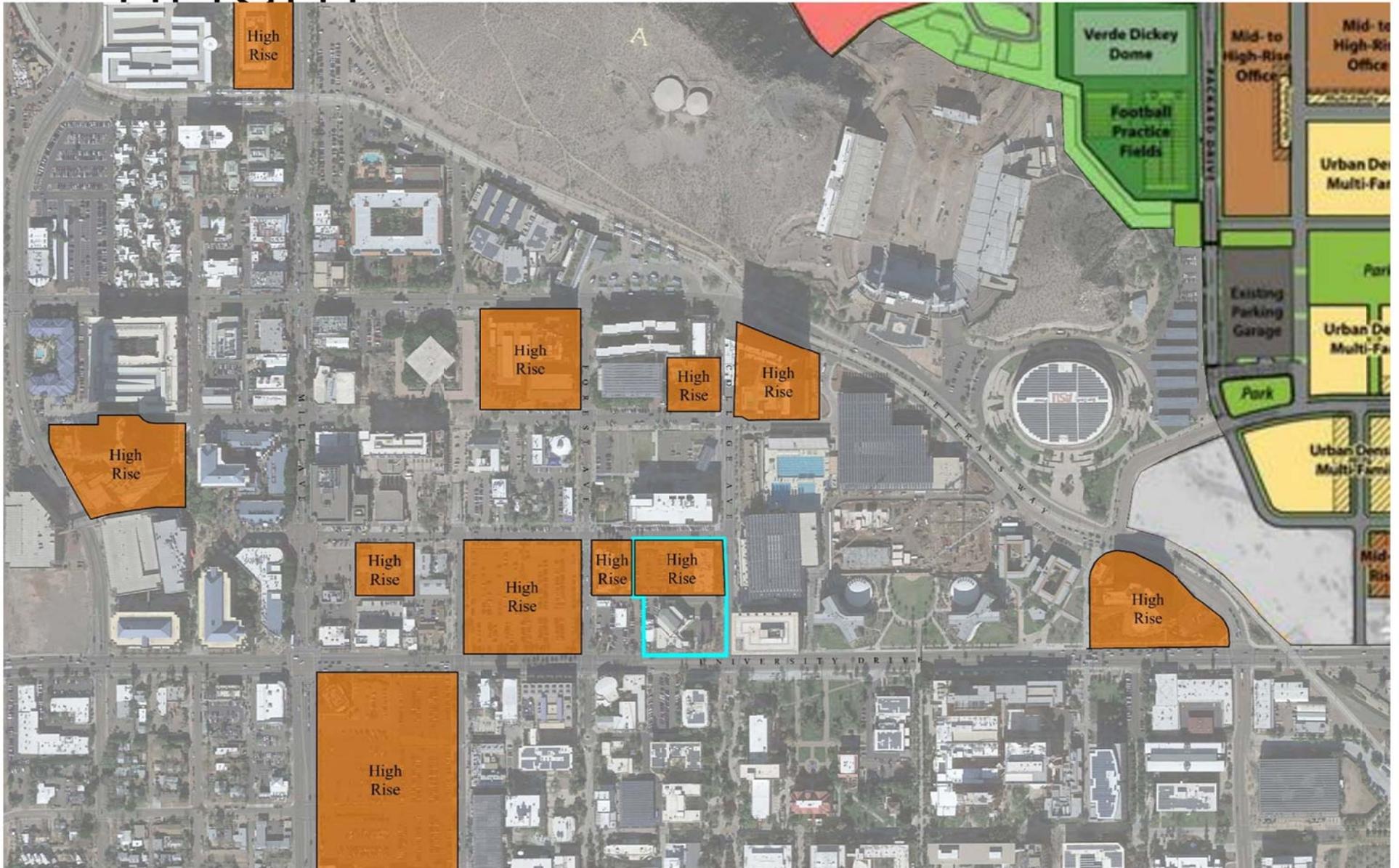


3D Master Plan enlargement



ATTACHMENT 161

HEIGHT

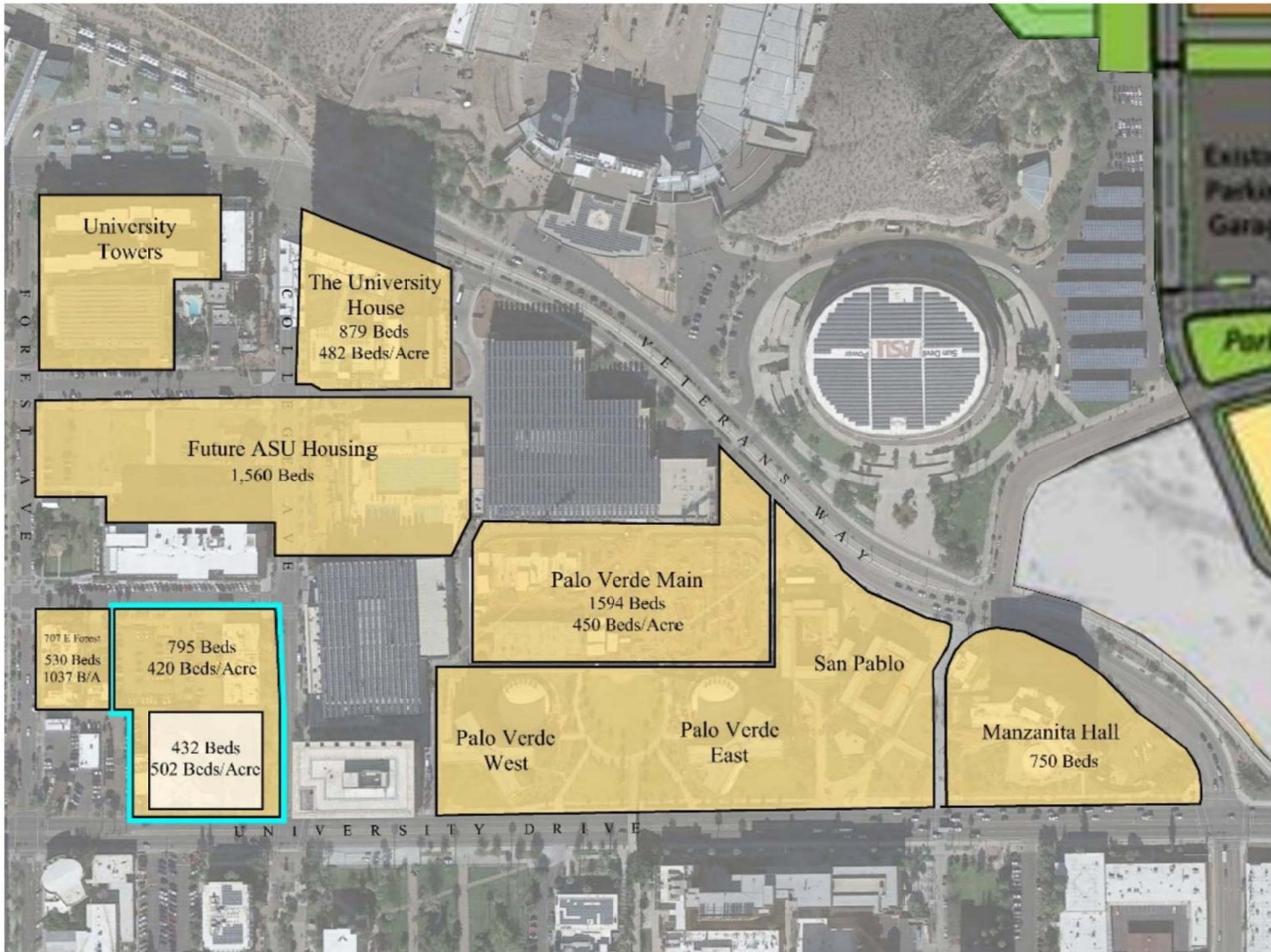


General Plan Density



UNITS









Our Portfolio

206

student properties

65,000

units

113,500

beds

75

multi-family properties

15,500

units



UCF Foundation, Inc.

- 2,532 bed property in Orlando, Florida. The property consists of 34 buildings and 3 clubhouses

To whom it may concern:

Asset Campus Housing assumed management of our two student housing properties in 2011. It was a very challenging period when major renovations were underway. Not only was ACH able to effectively manage the properties during this multi-year improvement phase, but under ACH's leadership, tenant occupancy and tenant satisfaction at both properties increased!

Our working relationship with ACH management has always been open and honest, regardless of the subject and ACH continues to be responsive to all our inquiries and requests. In addition, ACH has always demonstrated a willingness to react and adapt to the changing issues facing our students, our university and our community. We look forward to building on our successful partnership with them for years to come.

I would recommend them to manage any university affiliated housing activities.

Sincerely,

A handwritten signature in black ink that reads "Bert Francis".

Bert Francis, fl, CPA
Chief Financial Officer

Department of Accounting & Financial Management Services
12424 Research Parkway, Suite 140 • Orlando, FL 32826-3257 • (407) 882-1220 • Fax (407) 882-2866

An Equal Opportunity and Affirmative Action Institution



Our Accomplishments

**Inc.
5000**

2011
2012
2013
2014
2015

2013
Inc.
Hire Power
Awards
#13

#1

Top 10 Real
Estate Companies

#1

Top 10 Houston
Companies

**NMHC
50**

**STUDENT HOUSING
BUSINESS**

Largest 3rd Party Student Housing
Management Company

101
BEST AND
BRIGHTEST
COMPANIES
TO WORK FOR

**STUDENT HOUSING
BUSINESS
INNOVATOR AWARDS**

MFE 50
TOP
SURVEY
MULTIFAMILY EXECUTIVE

HOUSTON BUSINESS JOURNAL
BEST
PLACES TO WORK

FIRST ANNUAL
**BEST PLACES TO
WORK** IN MULTIFAMILY



HBJ 2015
FAST 100
ENTERPRISE CHAMPIONS

REVISED CONDITIONS SUPPORTED BY APPLICANT:

PAD CONDITIONS:

10. The plans shall be modified to provide ~~the a~~ minimum **required 444** vehicle parking spaces **pursuant to Section 4-607, Downtown Parking Standards**, ~~which excludes those~~ **excluding spaces** required for the existing church use. The ~~444~~ spaces may be accomplished with ~~below grade~~ parking in the **on-site** garage or off-site parking with the recordation of a parking affidavit.
11. The parking study and/or PAD shall be revised as follows:
 - c. ~~Provide an updated letter from ASU or another property owner in the vicinity of the project that specifies the reservation of a minimum of 72 vehicle parking spaces for the church use.~~ **The off-site parking agreement intended to serve the 72 spaces required for church use shall be maintained. Any changes to the off-site parking shall require the church to resubmit to the City and provide authorization from another property for shared use parking.**

DPR CONDITIONS:

8. The developer shall provide a south-bound dedicated right-turn lane on College Avenue. ~~The Lane shall not extend north of the alley and will follow an approximate alignment with the original street curb. Final details shall be approved by the Public Works Department Transportation Division.~~ **Details of the turn lane configuration and dimensions shall be consistent with Exhibit C-1, dated 5-11-16. Such improvements shall be completed prior to receiving certificate of occupancy, subject to review by the Public Works Department Transportation Division.**
16. Parking Garage:
 - b. ~~At the ends of dead-end drive aisles in locations currently shown on the plans extending greater than~~ **80'**, provide a designated turn-around space, minimum 8'-6" clear in width (locate on left side if available), including 3'-0" vehicular maneuvering area for exiting. Turn-around area shall be clearly demarcated.